



SA TOMORROW SUB-AREA PLANNING: HIGHWAY 151 AND LOOP 1604 AREA REGIONAL CENTER PLAN

ADMINISTRATIVE DRAFT: FEBRUARY 1, 2022

THIS IS A PROPOSED DRAFT SUBJECT TO COMMUNITY INPUT, LEGAL AND ADMINISTRATIVE REVIEW,
AND WILL BE REPLACED BY THE FINAL PLAN ADOPTED BY THE CITY COUNCIL.

[With Staff-recommended tracked-changes as of March 1, 2022.](#)

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1 Introduction

Process and Timeline

The process of developing the Highway 151 and Loop 1604 Area Regional Center Plan was approximately 3.5 years from project chartering to City Council adoption. Planning Department staff worked with a wide range of community members that included neighborhood organizations and representatives, business and property owners, employers, educational and medical institutions, and partner organizations and City departments to create a realistic and implementable plan for this important regional center.

Phase 1: Project Chartering

August-December 2018:

The first phase of the project focused on project chartering. This phase ensures the planning process has a clear timeline and supports the overall goals of SA Tomorrow, while identifying key stakeholders to be involved in the process. Project chartering also included determining the Planning Team membership, finalizing the detailed plan area boundaries, and refining projections for future housing, job, and population growth within all Regional Centers and high-capacity transit corridors.

Phase 2: Analysis and Visioning

January-March 2019:

The second phase of the project focused on assessing the existing conditions and growth capacity of the Highway 151 and Loop 1604 Area Regional Center, while drafting a vision and set of goals for the future of the area with Planning Team and community direction. The analysis and refinement of existing conditions helped ensure that the vision and goals for the Highway 151 and Loop 1604 Area Regional Center Plan are grounded in the proper context.

Phase 3: Plan Framework

April 2019-April 2021:

The third phase of the project focused on working with the community and stakeholders to establish the Plan Framework. The Plan Framework components include Land Use, Focus Areas, Mobility, Amenities and Public Space, Housing, and Economic Development.

Phase 4: Recommendation and Implementation Strategies

June 2019-April 2021:

The fourth phase developed specific projects, programs, and policies to effect change in the Highway 151 and Loop 1604 Area Regional Center. This phase also included the development of specific, action-oriented implementation strategies and recommendations for potential funding sources.

Phase 5: Documentation and Adoption

May 2021-March 2022:

The last phase of the project was devoted to converting the project website into the final ePlan for the Highway 151 and Loop 1604 Area Regional Center Plan, creating the Executive Summary, and guiding

the plan through the approval and adoption process. The Project Team met with City departments and other partners to develop critical next steps to support implementation of the plan.

Stakeholders

The Highway 151 and Loop 1604 Area Regional Center Plan planning process included a range of engagement activities such as interviews, workshops, meetings, town hall events, and community meetings with stakeholders from the following groups:

- Alamo Area Council of Governments (AACOG)
- Alamo Area Metropolitan Planning Organization (AAMPO)
- Brown & Ortiz, P.C.
- Capital Group
- CHRISTUS Santa Rosa Health System
- City Council District 6
- City of San Antonio Center City Development & Operations Department (CCDO)
- City of San Antonio Development Services Department (DSD)
- City of San Antonio Economic Development Department (EDD)
- City of San Antonio Metropolitan Health District (Metro Health)
- City of San Antonio Neighborhood & Housing Services Department (NHSD)
- City of San Antonio Office of Innovation
- City of San Antonio Office of Sustainability
- City of San Antonio Parks & Recreation Department
- City of San Antonio Public Works Department
- City of San Antonio Transportation Department
- Frost Bank
- GM Financial
- Hyatt Regency Hill Country Resort and Spa
- Kohl's, Risk Management Operations
- Methodist Healthcare System
- Mirabella at Legacy Trails
- Mountain View Acres Neighborhood Coalition
- North San Antonio Hills Homeowner's Association
- Northside Independent School District (NISD)
- Northwest Vista College
- Oak Creek Neighborhood Association
- San Antonio River Authority (SARA)
- San Antonio Water System (SAWS)
- SeaWorld San Antonio
- Stonegate Hills at Westover Hill Homeowner's Association
- Texas Department of Transportation (TxDOT)
- VIA Metropolitan Transit (VIA)
- Wells Fargo
- Westover Valley HOA

The Planning Team

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- Gregory Beal, Frost Bank
- John Hadden, Oak Creek Neighborhood Association
- Joe Hindsley, Hyatt Regency Hill Country Resort and Spa
- John Wright, Mountain View Acres Neighborhood Coalition
- Joshua Spencer, SARA
- Kenneth Brown, Brown & Ortiz, P.C.
- Laura Cabanilla, Wells Fargo
- Melanie Forde, SeaWorld San Antonio
- Mike Tavitas, Stonegate Hill at Westover Hills
- Pamela Mote, CHRISTUS Santa Rosa Health System
- Richard Ramos, North San Antonio Hills
- Santiago Jaramillo, VIA Metropolitan Transit
- Sarah Edwards, Capital Group
- Susan Handlin, Stonegate Hill at Westover Hills
- Taris Bell, Westover Valley HOA
- Tom Lehrman, GM Financial
- Valerie Cobos, Frost Bank

The Selection Process

Each of the SA Tomorrow Sub-Area Plans was developed with regular input and participation from residents, business owners, property owners, institutional representatives, and other key partners and stakeholders. In addition, a formal Planning Team was created for each sub-area that provided more frequent, in-depth, and consistent advice and guidance throughout the planning process. The composition of the Planning Team for each area is drawn from the representatives and stakeholders described above, and varies depending on the existing uses, assets, challenges, and opportunities associated with each area. While the Highway 151 and Loop 1604 Area Regional Center Planning Team list does not include all who were invited to participate, it does include those who served as alternate and replacement representatives for participating organizations.

2 Existing Conditions

A History of the Highway 151 and Loop 1604 Area

From the 1800s to today, this area has transformed dramatically from ranchland to recreation to a regional center of economic activity. Spurred by the efforts of one master developer, the Highway 151 and Loop 1604 Area Regional Center is now home to a variety of uses, including large employers, major institutions, and new residential development.

1800s to 1970s: Ranchland

In the late 1800s, San Antonio's city limits were miles away from this sprawling ranchland. The Rogers family had traded land in Wisconsin for 2,000 acres outside of San Antonio, and ran the area as a cattle ranch. Eventually, the Rogers family combined with the Wiseman family (two streets in the area are named after these first developers), creating the Rogers-Wiseman Ranch which was in operation for many more decades. The area started shifting, and the City of San Antonio started sprawling further outward. In the 1950s, when planning for Loop 410 was underway, there were very few residential developments in the area except for a few houses in the Mountain View Acres neighborhood which had dirt roads at the time. By the 1970s, there were plans for Loop 1604, and a few more houses in the area in the neighborhoods of North San Antonio Hills and Oak Creek.

1980s to early 1990s: Infrastructure and Tourism

The area was still outside of the city limits until the 1980s when it caught the eye of local developer, Charles Martin "Marty" Wender. Seeing potential in the area, over several years he amassed 3,500 acres outside of the city limits from Potranco Road to just below Culebra Road, and began installing necessary infrastructure and utilities for his "Westover Hills" development. He also proposed and donated land for a major highway through the area, Highway 151, to connect with U.S. Highway 90 and Loop 1604. This highway would support new development, such as the budding plans for SeaWorld San Antonio. On May 27, 1988, SeaWorld San Antonio welcomed 75,000 visitors on opening day, becoming a regional and national attraction. In 1993, the Hyatt Regency Hill Country Resort and Spa opened, and now incorporates the history of the Roger-Wiseman Ranch into their hotel's décor.

Late 1990s to 2000s: Economic and Residential Development

In 1998, Northwest Vista College opened, and now serves thousands of students each year aiming for higher education. By 2004, all four lanes of Highway 151 were completed and in 2005 a Gateway Corridor overlay zoning district was adopted to ensure cohesive development design around the corridor. In 2006, the first Alamo Ranch development broke ground outside of the city limits, bringing more growth to the area. In 2009, CHRISTUS Santa Rosa Hospital – Westover Hills opened, pulling more medical offices toward the area, while other commercial campuses were developing as well. The area was quickly finding its footing in the market as a place with affordable land for the business, health, and technology sectors.

2010 to today: Rapid Growth and Economic Expansion

Since 2010, the area has grown considerably, both within and outside the city limits. In 2014, funds were approved to expand Loop 1604 south from Highway 151 to U.S. Highway 90, a project which is currently under construction, since traffic has increased considerably in the area. In addition to the corporate campuses and medical offices developed in the early 2000s, the area has become a prime location for

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data centers. San Antonio is a safer choice for data centers and server farms due to our relatively low chance for major natural disasters, and this area in particular is well-suited since it has reliable electricity and high-speed internet and is removed from freight train and airspace vibrations. Companies such as GM Financial, Microsoft, QVC, and Kohl's have placed their data hubs in the area.

[See **Exhibit 1: Existing Conditions Atlas**]

[See **Figure 1: Plan Location Map**]

[See **Figure 2: Study Area Map**]

Assets

The Highway 151 and Loop 1604 Area has a strong identity, especially through the creation of the Highway 151 Gateway Corridor overlay district in 2005. This overlay has ensured that the area maintained many of its trees even with considerable development. With two major highways serving the regional center, there is easy access from other parts of the city and county for people to live, work, and play within the area. For business owners, there is strong, reliable fiber internet and electricity infrastructure – a crucial reason why so many companies have located their call centers, data storage, and operations in the area.

Because of many of the assets listed above, the regional center has a diverse mix of institutional and commercial assets. The area has been a significant site for tourism – especially with SeaWorld San Antonio and the Hyatt Regency Hill Country Resort and Spa. There are increasingly more medical offices and senior living/long-term care centers, anchored by the CHRISTUS Santa Rosa Hospital – Westover Hills and Methodist Hospital that is under construction as of the writing of this plan. Many other major financial employers such as Capital Group, Wells Fargo, GM Financial, and Frost Bank have also made the area their home. Northwest Vista College is a considerable asset to the area, and has partnered with other area-landowners, such as Microsoft, for educational opportunities that also meet the employer's needs.

Challenges

As the area continues to grow, the most significant challenges described by residents and employers alike largely center around transportation. Traffic was a major concern for many, which could be alleviated with the planned TxDOT expansion of Highway 151. The intersection of Culebra Road and Loop 1604 was also mentioned as a major pressure point. From a high-level planning perspective, the development pattern of the existing road infrastructure in the area is not necessarily conducive to a transit network, especially with the difficulty for people to walk from inside larger campus developments or subdivisions to the transit network. Drainage is also a considerable concern. As more impervious surface covers the area, it poses issues both for residents and for endangered karst invertebrate species underground. Residents have mentioned flooding following new development, which could be mitigated through increased green infrastructure.

Opportunities

The Highway 151 and Loop 1604 Area is one of the city's fastest growing regional centers and has a considerable amount of vacant land, which presents opportunities for development that meets the needs of existing and future residents, employees, and students. Many employers have mentioned that

there is room to grow, and relatively affordable land in the area. While this is an opportunity, it can also present a challenge for how the area changes in the future. While transportation is a challenge, there are also many opportunities for existing business owners to coordinate to meet their employees' needs perhaps through rideshare services or other on-demand services. Affordable housing is needed throughout the city, and the regional center could also accommodate some of that growth as well, especially if well-coordinated with transportation. As the area grows, there is desperate need for park, trail, and public open space. Areas within the floodplain could be an opportunity to meet this need. There are many opportunities for the area to become more walkable, and have more amenities to serve residents, employees, and students in the area.

Sub-Area Plans and Existing Neighborhood and Community Plans

Sub-Area Plans are intended to provide a coordinated, efficient, and effective approach to planning in San Antonio. In contrast to other regional centers and community areas, there are no existing adopted Neighborhood or Community Plans to incorporate into the Highway 151 and Loop 1604 Area Regional Center Plan. Representatives from several registered neighborhood associations in the area were able to sit on the Planning Team and guide resident and neighborhood priorities for the plan.

3 Vision and Goals

What is a Vision Statement?

A vision statement describes the desired state of a place in the future. With community support, an effective vision can influence decisions and inspire action to move toward that idealized future. Goals further describe the outcomes that will support the realization of the vision. These, in turn, are supported by more specific strategies and actions that will implement the bigger-picture vision and goals. These strategies will involve specific proposed projects, programs, policies, and other means of achieving the community vision.

The Highway 151 and Loop 1604 Area Regional Center Plan Vision and Goals were developed with input from residents, community stakeholders, and Planning Team members through an iterative process of developing and refining these concepts. During preliminary community engagement efforts and Planning Team meetings, community members, employers, landowners, residents, students, and major institutions articulated important values and identified the regional center's assets, challenges, and opportunities. This community input became the guiding principle for the Highway 151 and Loop 1604 Area Regional Center Plan Vision and Goals, which were refined with feedback from the Planning Team and participants at the second Community Meeting.

Establishing the Vision and Goals

The success of the SA Tomorrow Sub-Area Plans depends on broad participation from area stakeholders. To ensure this success, City staff worked with a wide range of community members throughout the planning process for the Highway 151 and Loop 1604 Area Regional Center Plan. These included neighborhood associations, business and property owners, residents, employers, educational and cultural institutions, public and non-profit organizations, and other City departments to create a realistic and implementable plan for the Highway 151 and Loop 1604 Area.

The planning process was designed to create a “feedback loop” between the City and the community as the plan was developed. This approach ensures that the Plan reflects community values and priorities. A variety of tools and techniques were used to ensure that those interested were well-informed about the Highway 151 and Loop 1604 Regional Center; encouraged to participate in a range of stimulating events and activities; and engaged in providing constructive feedback on a preferred future.

For each public input exercise, this document describes what was asked, how the input was presented back to the stakeholders, and how results were carried forward in further engagement exercises and eventually incorporated into the plan. Results from the exercises and surveys are available in the website Documents Library.

To facilitate public information and community participation, the Highway 151 and Loop 1604 Area Regional Center Plan website was created and made available to the general public. PowerPoint presentations, meeting summaries, and other materials were made available to the public on the website. The website includes a section for leaving comments which are sent directly to the project manager.

Below are descriptions of key exercises that were accomplished in order to draft and refine the vision and goals for the future of the Highway 151 and Loop 1604 Area Regional Center Plan. Community feedback was gathered through two community meetings and two online questionnaires. The feedback from this community engagement was presented and discussed at four planning team meetings, where Planning Team members reviewed and refined drafts. Throughout the planning process, the vision and goals were continually revisited to ensure recommendations, strategies, and other decisions were consistent with the long-term vision for this area.

Exercise 1: Draft Assets, Challenges, and Opportunities

At the first Planning Team meeting, the group discussed assets, challenges, and opportunities in the Highway 151 and Loop 1604 Area, as well as strategies for outreach efforts. This information helped inform draft plan vision elements, priorities, and goals.

Exercise 2: Draft Vision Elements

During the second meeting, Planning Team members began developing the vision and goals for the area. Participants were asked what major themes, opportunities, and issues the plan should address. Planning staff facilitated a discussion on the major themes so Planning Team members could openly discuss and elaborate on key aspects. The results of the discussion were recorded on large pieces of paper in front of the Planning Team. Results of Exercise 2 were presented to the Planning Team at the next meeting.

Exercise 3: Develop Draft Area Vision and Goals

During the first Highway 151 and Loop 1604 Area Community Meeting, attendees participated in an open house wall graphic exercise, identifying assets, challenges, opportunities, and potential vision and goal elements. To provide an alternative to attending the Community Meeting, an online questionnaire was also available, inviting participants to identify assets, challenges, opportunities, threats, and potential vision and goal elements. The questionnaire asked open-ended questions about what elements of their community they like, dislike, want to change, or want to keep. Results of Exercise 3 were presented to the Planning Team at the next meeting.

Exercise 4: Review Draft Vision and Goals

During Planning Team Meeting #3, the initial draft vision statement and goals were presented for discussion and refinement while considering comments that were provided by the broader public in the first Community Meeting and Vision and Goals online questionnaire.

Exercise 5: Review and Confirm Draft Vision and Goals

During Planning Team meeting #4 the revised vision and goals were reviewed for approval by the Planning Team. The draft vision and goals were then posted to the plan website.

Exercise 6: Finalize Draft Vision and Goals

The draft vision and goals were presented at the second Community Meeting on large sheets of paper, with instruction on how to use stickers and markers to suggest improvements or confirm agreement. Feedback from attendees was strongly encouraged and discussed. The participants welcomed and supported the draft vision and goals. Similar to the first Community Meeting, an online questionnaire was also available which asked residents and stakeholders whether they agreed or disagreed with the draft vision and goals. The questionnaire also allowed space for comments.

The results of the public visioning process are presented below. The vision and goals were referenced throughout the planning process to ensure key recommendations and strategies aligned with the long-term vision for the area.

Vision

The Highway 151 and Loop 1604 Area will continue to thrive as a center where people live, work, learn, and play. The area stands out as a place to experience the Texas Hill Country feel, but also breathes new life as a place where businesses, shopping, and tourism thrive. In the future, there will be more opportunities for those who live or spend time in the area to recreate outside, as well as new opportunities for mixed-use nodes where people can gather. Future growth will respect and work to maintain the green space and natural feel of the area, while ensuring strong connections inside and outside of the city through new transportation options as well as efficient major thoroughfares.

Goals

Goal 1: *Create a cohesive identity for the area while preserving Hill Country character.*

Goal 2: *Support new mobility choices and manage congestion on major roadways while encouraging new development and land use choices that do not increase congestion.*

Goal 3: *Leverage education, infrastructure, and business assets to support future economic growth.*

Goal 4: *Create new public open spaces, recreation options, and other mixed-use places for people to gather or play.*

Goal 5: *Provide a diverse array of housing options for all stages of a resident's life.*

4 Plan Framework

[See **Figure 3: Plan Framework Map**]

The Plan Framework comprises the six main topics of the plan: Land Use, Focus Areas, Mobility, Amenities and Public Space, Housing, and Economic Development. Detailed background information, maps, illustrations, and recommendations for each of those topics can be found in their respective sections following this overview. Throughout those sections, numerous important recommendations and strategies are identified for achieving the vision and goals of the plan based on existing conditions analysis; research and best practices case studies; collaboration with other City departments and partner agencies and organizations; and many conversations with the Planning Team and the wider Highway 151 and Loop 1604 Area community.

Implementation Priorities

While all the recommendations and strategies in the plan are important, this overview highlights those that are either most critical to achieving the plan vision and goals, are more likely to be funded and implemented in the short-term, build upon other existing or ongoing initiatives, or are a necessary first step to provide a foundation for other projects and investments. These Implementation Priorities are presented in two ways: the list below represents the full range of priorities, while the accompanying Plan Framework Map illustrates all those that can be physically depicted in order to show important overlaps, adjacencies, and mutually supportive relationships. Some Land Use, Housing, Economic Development, and other recommendations and strategies are not easily depicted on the map but are included in the Implementation Priorities list. Together, the Implementation Priorities and Plan Framework Map represent the key strategic concepts and physical improvements that will influence future development of the Highway 151 and Loop 1604 Area Regional Center.

Land Use

Strategy 1.1 (Regulatory and Policy)

Review current zoning to ensure that the zoning within established neighborhoods is consistent with existing Residential Estate, Low Density Residential, and Urban Low Density Residential uses and future land use designations. Rezone properties that are not zoned consistently to ensure that undeveloped lots are not over-built in the future.

Strategy 2.1 (Regulatory and Policy)

Review current zoning for consistency with the patterns in development intensity reflected in the future land use map. In particular, ensure that properties in areas designated Regional Mixed-Use or Business/Innovation Mixed-Use have been zoned to accommodate the range of uses anticipated for these respective land use categories.

Strategy 3.2 (Investment)

Identify and acquire available land located within stream corridor buffers, floodplain areas or land with steep slopes in order to provide public greenbelts, trails, and natural areas, particularly connecting Leon Creek Greenway through Culebra Creek.

Strategy 3.5 (Regulatory and Policy)

For new nonresidential development, encourage the use of land available through setbacks, tree

preservation requirements, floodplain and stormwater protection areas, and steep slopes to be designated for trails and publicly accessible open spaces.

Focus Areas

Strategy 1.1 (Regulatory and Policy, Partnerships, Investment)

Explore creation of development incentives to encourage parks, plazas, or other open space in new private developments in regional centers that lack adequate park space.

Strategy 2.1 (Regulatory and Policy, Partnerships)

Explore expanding the Highway 151 Gateway Corridor, or adopting additional Corridor Overlay Districts for other major corridors or the focus areas within the regional center.

Strategy 2.2 (Regulatory and Policy, Partnerships, Investment)

Coordinate with the San Antonio River Authority (SARA) to identify priority areas for green infrastructure and prioritize projects that encourage green infrastructure and other natural stormwater best management practices.

Strategy 3.2 (Regulatory and Policy, Partnerships, Investment)

Coordinate with VIA Metropolitan Transit to explore opportunities for transit connections within the regional center and identified focus areas.

Mobility

Strategy 1.1 (Regulatory and Policy, Investment)

Continue evaluating and implementing proven strategies and best-practice improvements, to potentially include traffic calming and “complete streets” principles, which improve pedestrian, bicycle, and traffic safety and help achieve San Antonio’s Vision Zero goals. The highest priority area is the identified Severe Pedestrian Injury Area (SPIA) on Potranco Road from Loop 1604 to Rousseau Street.

Additional analysis of pedestrian, bicycle, and vehicle crash data and community input, also identified the following as priorities:

- Loop 1604 and Culebra Road; and
- Culebra Road and Easterling.

Strategy 3.2 (Regulatory and Policy, Partnerships)

Support partnerships that provide a rideshare program or on-demand circulator between major employer campuses and major transit stops.

Strategy 4.1 (Regulatory and Policy, Partnerships, Investment)

Implement first/last mile strategies, such as sidewalks, curb ramps, crosswalks, bicycle facilities, etc., at VIA Metropolitan Transit facilities, to promote access to transit by creating inviting, quality public space around transit facilities, stations, and stops where large numbers of people can benefit. The priority location for these investments is near Northwest Vista College on Wiseman Boulevard between North Ellison Drive and Highway 151.

Amenities and Public Space

Strategy 1.1 (Partnerships, Investment)

Coordinate with the Parks & Recreation Department to ensure completion of the Leon Creek Greenway system through Culebra Creek.

Strategy 1.2 (Regulatory and Policy, Partnerships, Investment)

Explore the creation of development incentives and expanded park dedication requirements to encourage projects that increase publicly accessible open space (parks, plazas, trails, etc.) within the regional center, especially in areas designated for Regional Mixed-Use or Business/Innovation Mixed-Use.

Strategy 1.3 (Regulatory and Policy, Partnerships, Investment)

Work with the Parks & Recreation Department to more fully convey opportunities for property owners and developers to dedicate land for parks, plazas, or other recreation space coordinated with the Parks & Recreation Department's mandates.

Housing

Strategy 1.1 (Regulatory and Policy, Partnerships)

Work with vacant property owners and developers to integrate multi-family developments into larger mixed-use projects or areas. Require projects to orient to streets and pedestrian paths and provide connections to adjacent and nearby commercial areas.

Strategy 1.2 (Regulatory and Policy, Partnerships)

Promote higher density multi-family housing in a mixed-use environment near VIA Metropolitan Transit bus infrastructure to support transit-oriented development.

Strategy 2.1 (Regulatory and Policy, Partnerships)

Partner with local affordable housing providers and developers to identify potential sites and projects for development of affordable and mixed-income housing. Prioritize sites in focus areas and along mixed-use corridors. Consider proactively purchasing sites for future affordable housing development through the use of a community land trust, and/or through partnership with San Antonio Housing Authority (SAHA) and other non-profit housing partners to ensure affordable units are available in the area.

Economic Development

Strategy 1.2 (Partnerships)

Support partnerships and programs between Northwest Vista College and area businesses to create and tailor opportunities for workforce education and training that support target industry businesses in the regional center.

Strategy 2.1 (Regulatory and Policy)

Encourage the creation of mixed-use centers along Highway 151 with retail and entertainment uses oriented to the plan area workforce and residents. Explore potential development incentives (such as density bonus and other regulatory benefits) for mixed-use developments that provide public gathering spaces, entertainment attractions, and/or increased mobility options and connectivity.

Plan Framework Map Overview

[See Figure 3: Plan Framework Map]

The Plan Framework Map identifies and shows the interrelatedness of key physical concepts and strategies in the plan. These include the focus areas, recommended modal priorities that would then help determine appropriate streetscape improvements, enhanced trail connections, pedestrian crossings, and mobility hubs. These recommended physical improvements and investments are complemented by other supportive plan strategies related to land use, housing, economic development, and providing access to and preserving green space.

Trails and Open Space

The number one suggestion from residents was to increase open space within the Highway 151 and Loop 1604 Area Regional Center. This can be achieved through many means as described in the Implementation Priorities above. The SA Tomorrow Comprehensive Plan and the SA Parks System Plan have the goal of ensuring residents live within a 10-minute walk of a park. This area misses the mark. There are future plans to extend the Leon Creek Greenway through Culebra Creek, which is a high priority for the implementation of this plan. Additionally, there could be increased trail connections throughout the plan area. This could be facilitated with the help of property owners and future developments to connect to a railway system, perhaps using areas within floodplain to increase open space. This type of development coordination has happened in north San Antonio and could be a benefit to this area as it grows.

Increased Walkability

To ensure connectivity between the future Culebra Creek railway and existing residential areas and commercial campuses, increased investment in the walkability of the regional center is needed. The Plan Framework Map highlights several key mobility aspects – such as new multimodal connections, bicycle connections, and multi-use trails – that will further connect the existing assets of the regional center and create new assets. Additionally, several areas are identified for improved crossing enhancements. Increased activity and more people walking or biking in the plan area necessitates safer crossings to prevent pedestrian/bicycle/automobile collisions. Lastly, several pedestrian focus areas are identified as priority locations to improve safety and connection for those who might be walking in the area – from people casually taking a stroll to lunch to parents walking with their children to nearby trails.

Balanced/Multimodal Streets

The area is anticipated to become more mixed-use in the future, with residential areas nearby commercial areas. Given this, two balanced/multimodal priority corridors were selected as areas where all transit modes should work together in harmony – Rogers Road and North Ellison Drive. This means bus stops would be comfortable to access on foot, and vehicular traffic will be managed in a way to reduce risk to bicyclists and pedestrians. The specifics of how these streets will function in the future is not prescribed in this plan, but rather they are highlighted as necessary based on the anticipated future land use in the area. With more people living and working on these corridors, managing how people are able to walk, bike, drive, and take public transit will be essential.

Focus Areas

Focus areas are identified in every sub-area plan as areas that could see considerable change in the future. For the Highway 151 and Loop 1604 Area Regional Center, Focus Areas #1, #2, and #3 are large

vacant parcels, and Focus Areas #4 and #5 are priority multimodal corridors. In the Focus Area section of the Plan Framework, there are imagined renderings for the types of development or streetscape improvements that would benefit these areas. While the development will not occur exactly as designed, the focus areas were an exercise in capturing stakeholders' vision and guiding design principles for the future of the regional center.

Mobility Hubs

Employers and residents both would like to see increased transportation options available within the regional center. The areas identified as "Mobility Hubs" on the Plan Framework map would be good locations for concentrated transportation improvements based on the Future Land Use dedication of the area. As more people live and work in the regional center, providing increased transit opportunities will be essential. But, these must also be coordinated with increased options for walking, bicycling, scooters, etc., so people have several choices for safely getting from the bus stop to their final destination.

Establishing the Plan Framework and Recommendations

The Plan Framework includes recommendations and strategies around future land use; focus areas and mixed-use corridors for development or improvement; pedestrian, bicycle, and street improvements; amenities and public space; and priority areas to encourage mixed-use development. All sections of the Plan Framework are briefly described below.

Land Use: Land Use is a foundation of this plan and all the other sections are intended to respond to and support the designated future land development patterns. One of the key goals of implementing the SA Tomorrow Comprehensive Plan is to create a future land use map for the entire city. Through carrying out sub-area plans, such as the Highway 151 and Loop 1604 Area Regional Center Plan, the Planning Department will eventually create a detailed future land use map for the entire City of San Antonio.

Focus Areas: Focus areas are key areas where future investments or other improvements are desired. Mixed-use corridors are a type of focus area where different uses within the corridor are encouraged to generate activity. These mixed-use corridors are key places where there should be future improvements that encourage traveling by different modes other than car, such as walking, biking, and public transportation.

Many of the concepts within this plan are long-term and somewhat abstract. The Focus Areas section aims to take particular project ideas and ask which could really serve as a good example for the types of developments or improvements this area would like to see in the future? While this section has detailed renderings, the specific style choices are more for illustrative purposes to show potential, rather than prescribe certain aspects of a development.

Mobility: Getting around in the future is, understandably, a key concern for future growth. This section suggests strategies and future improvements to help the plan area thrive in the future, instead of becoming more and more congested.

Amenities and Public Space: As the city grows, we need to ensure all communities have things that make them not only nice places to live or work, but also places where people enjoy time outdoors, cultural assets, and basic infrastructure. This section describes desired future projects or policies to make that future a reality.

Housing: In addressing future growth, there has to be a discussion of housing. This section supports the concepts within the future land use section with specific recommendations for housing in the plan area.

Economic Development: Future growth will also depend on economic opportunities within an area. This section describes specific recommendations around how to create an area with a thriving economy.

Public Feedback

The Highway 151 and Loop 1604 Area Regional Center Plan Framework was developed through a combination of technical analysis and community input. The Framework illustrates and outlines the overall long-term vision for the Highway 151 and Loop 1604 Area Regional Center, including future land use types, priority areas where new development may be focused, recommendations for key mobility improvements, opportunities for additional parks and open space, and other “big moves” or ideas that will shape the future of the area.

At the beginning of the planning process, the project team developed an in-depth study and analysis of the Highway 151 and Loop 1604 Area Regional Center to understand the history and development of the area as well as the existing conditions. The Planning Team provided their input on the area’s existing assets, challenges, and opportunities to develop a better understanding of the area and the community’s priorities and values. City staff also organized and facilitated additional stakeholder input and public outreach to capture a broad range of area residents’ considerations. Through a series of facilitated work sessions and interactive exercises, the Planning Team provided input and direction that is reflected in the Plan Framework and throughout all sections of the Highway 151 and Loop 1604 Area Regional Center Plan.

Over several months, City staff and the Planning Team worked collaboratively to build upon the Framework to identify the key priorities, improvements, and strategies that will guide growth, development, and investment in the Highway 151 and Loop 1604 Area. A series of draft recommendations on several topics were developed for stakeholder feedback and are reflected in the Plan.

During the second Planning Team meeting, staff presented on existing conditions and the team discussed assets, challenges, and opportunities in the Highway 151 and Loop 1604 Area Regional Center.

The Plan Framework was then iteratively developed, based on Planning Team conversations, other public input, and analysis on each of the plan sections. Multiple Planning Team meetings were devoted to each of the topics, allowing for initial feedback and then review of draft recommendations. Specific meetings and activities that contributed to the development of each Plan Framework topic are described at the beginning of each of those plan sections.

Land Use

The future land use element of the plan was primarily informed by input from Planning Team Meetings #4 and #5, and feedback received from the broader public in the first and second Community Meetings and associated online questionnaires. Initial public input for the draft vision and goals, and other public input related to housing, economic development, and other sections of the plan also informed the future land use element of the plan.

Housing

The housing recommendations were primarily informed by input from Planning Team Meetings #6 and #7, and feedback received from the broader public in the first and second Community Meetings, and associated online questionnaires. Initial public input for the draft vision and goals, and other public input related to future land use, economic development, and other sections of the plan also informed the housing recommendations and strategies.

Economic Development

In addition to discussing housing in Meeting #6, the Planning Team discussed preliminary economic development concepts and strategies. The Project Team presented information on economic development issues and trends in the area. Following the presentation, the Planning Team discussed economic strengths, opportunities, and weaknesses in the area.

In Meeting #7, the Planning Team discussed and provided feedback on economic development case studies and conceptual draft economic development recommendations that were based on discussion in the prior meeting.

The project team used the results of Planning Team Meetings #6 and #7 to create draft economic development recommendations. These were presented in the second Community Meeting and associated online questionnaire for feedback from the public.

The project team improved the draft housing recommendations, and created implementation strategies associated with each recommendation. These were presented for feedback in the second Community Meeting. This feedback was used to confirm and make final improvements to the economic development recommendations and strategies.

Mobility

During the second Community Meeting, the project team gathered input related to mobility. A board with mobility-related questions was displayed at the community meeting for attendees to respond to and provide additional comments and input. Based on input from attendees and input from the Planning Team during two meetings focused on mobility, major corridors were identified on the mobility framework diagram with modal priorities. The Mobility Framework Map was presented to and discussed with the Planning Team to determine appropriate modal priorities for roadways in the regional center. Trails, transit routes, bicycle routes, streetscape improvements, pedestrian safety, and traffic congestion were discussed in order to ensure that the Mobility Framework Map reflected the appropriate modal priorities and needs of the community.

Amenities and Public Space

The Planning Team also discussed amenities and public space at Meeting #9. Meeting participants were first briefed by City staff and San Antonio River Authority (SARA) staff on what elements make up a complete neighborhood such as active recreation opportunities, natural features, social spaces, and stormwater management, among other things.

After seeing examples of the elements that create complete neighborhoods, Planning Team members discussed which elements were most critical and should be prioritized to help achieve the vision and goals of the Highway 151 and Loop 1604 Area Regional Center Plan. After the presentation on complete neighborhoods, the team broke out into two groups for a mapping exercise to identify areas for amenities and pedestrian improvements, followed by a review of the exercise results.

Land Use

[See Figure 4: Future Land Use Map]

Future Land Use

The Highway 151 and Loop 1604 Area Regional Center Land Use Plan supports the [SA Tomorrow Comprehensive Plan, Multimodal Transportation Plan, and Sustainability Plan](#). It also draws on recommendations from the [SA Corridors Strategic Framework Plan](#) and implements the Vision, Goals, and Plan Framework for the Highway 151 and Loop 1604 Area. The Future Land Use Plan encourages growth and increased density at various scales in mixed-use centers and focus areas and along key transit and community corridors.

In order to achieve the vision and goals of this regional center plan, a comprehensive future land use map must be adopted to designate where future housing growth, commercial activity, and other uses are recommended in the area. As shown in the map, the Highway 151 and Loop 1604 Area should continue to accommodate robust commercial activity, while also allowing for more mixed-use development and respecting established single-family neighborhoods. Due to the significant amount of vacant land in this area, there is considerable opportunity to focus new development in appropriate places while also encouraging the creation of future opportunities for parks, trails, and open space in order to maintain the natural feel.

The following sections describe the general future land use patterns of the Highway 151 and Loop 1604 Area. Recommendations for implementing the land use plan follow, and the full catalogue of land use categories (including descriptions and allowable zoning districts) adopted in the Unified Development Code (UDC) are found at the end of this section.

[Residential Areas](#)

Within this regional center, existing single-family neighborhoods are designated Residential Estate, Low Density Residential, or Urban Low Density Residential based on their existing zoning, and how that aligns with the adopted future land use categories. The lowest density category, Residential Estate, designates the majority of the North San Antonio Hills Neighborhood. The second lowest density category, Low Density Residential, designates Oak Creek, Stonegate Hill, Westover Valley, Estates of Westover Hills, Creekside, Mirabella, and small portions of the North San Antonio Hills Neighborhood. Based on existing zoning, Urban Low Density Residential designates single-family homes north of Culebra Creek in the Reserve at Culebra Creek HOA. These land uses are not intended to change, and instead are intended to be preserved by this land use plan.

[Mixed-Use Centers and Corridors](#)

The Future Land Use Map includes specific areas for combined residential and commercial activity. This could mean that future development may have “vertical mixed-use” – where the ground floor might be commercial with residential above – or “horizontal mixed-use” – where one building on the parcel might be purely residential, and another might be purely commercial. These mixed-use areas are intended to allow for flexibility in development, and encourage more spaces where people can live, work, and play. The highest intensity of use is the Regional Mixed-Use category, followed by Urban Mixed-Use which accommodates medium intensity uses, and Neighborhood Mixed-Use for lower-intensity development.

Based on recent market activity and rezoning in this area, multi-family development is expected to increase. This is aligned with the SA Tomorrow Comprehensive Plan's goals to focus new housing growth within our regional centers. The areas designated as mixed-use are intended to accommodate this need, while also allowing commercial activity. Currently, most of these areas only allow commercial development, but with a mixed-use designation they will also allow for residential development, in accordance with observed recent rezoning and market activity.

Regional Mixed-Use

In the future, the Highway 151 and Loop 1604 Area would like to have more options where people may park once, or take transit, to mixed-use destinations where there may be shops, apartments, or other activities. This type of development can occur in areas designated Regional Mixed-Use. The vacant parcels at the intersection of Wiseman Boulevard and Highway 151 were highlighted as a key area for this kind of activity both because of its proximity to Northwest Vista College, and nearby connection to VIA Metropolitan Transit.

Regional Mixed-Use has also been designated for SeaWorld and the Hyatt Regency Hill Country Resort and Spa, due to their existing special zoning district of "ED" Entertainment District. The vacant parcels between these major anchor institutions could also see related development and have seen multi-family development in recent years. For this reason, these parcels are also designated as Regional Mixed-Use to allow for higher density residential and commercial development. Regional Mixed-Use is an appropriate designation for these areas due to their proximity to Highway 151, existing residential and commercial development, as well as their current zoning of "C-3" General Commercial District.

Urban Mixed-Use

Urban Mixed-Use areas will also allow for both commercial and residential development, but at lower densities and scale than Regional Mixed-Use areas. These areas have been identified along Culebra Road, Rogers Road, North Ellison Drive, and Westover Hills Boulevard. Many of these parcels are vacant and designating them as Urban Mixed-Use can help create a transition between existing commercial areas and more residential areas.

Neighborhood Mixed-Use

The lowest density/intensity mixed-use category, Neighborhood Mixed-Use, also allows for both residential and commercial, but helps "step down" the intensity of use and building scale toward more residential areas. This has been identified along Rogers Road toward the Mountain View Acres Neighborhood.

Commercial Areas

While many portions of the Highway 151 and Loop 1604 Area are designated as mixed-use to allow for both commercial and residential development, certain portions of the area are desired as strictly commercial. These areas are either already developed and not expected to change in the near future, or are fronting Loop 1604 and may be best-suited for commercial development.

Two categories of commercial are designated in the area: Regional Commercial (higher intensity) and Community Commercial (medium intensity). Regional Commercial is designated along both Loop 1604 and Culebra Road. Community Commercial is designated as a buffer to more residential areas, to allow a step down of uses away from major intersections. An example of this is the commercial area along Loop 1604 near the intersection with West Military Drive. The properties at the intersection are designated

Regional Commercial, and the properties between Loop 1604 and residential neighborhoods are designated Community Commercial.

Employment Areas

The [Employment/Flex Mixed-Use and Business/Innovation Mixed-Use](#) areas are intended to allow existing industries in this area to continue to locate here in the future, and to allow for both light manufacturing and commercial activity, helping to create a specialized economic hub in the Highway 151 and Loop 1604 Area Regional Center. Major employers have considerable corporate campuses within this regional center, in addition to anchor institutions such as Northwest Vista College, CHRISTUS Santa Rosa Hospital - Westover Hills, and a forthcoming Methodist Hospital. Several companies such as Microsoft and Valero have located data centers in the area, and other companies such as Capital Group and Kohl's have their call center operations located here. Additionally, high-tech manufacturers such as Joyson Safety Systems and Tower Semiconductor have their operations here. These types of uses are expected to increase in the area due to its robust electrical and fiber internet infrastructure. The [Employment/Flex Mixed-Use and Business/Innovation Mixed-Use](#) areas on the Future Land Use Map identify existing uses of this sort, and also highlight areas where this type of activity can be desirable in the future.

Parks and Open Space

Notably, no land is allocated for Parks/Open Space within this regional center. While there are plans to expand the Leon Creek Greenway through Culebra Creek, at the northern border of the regional center, adequate funding has not yet been secured for this site. Additionally, there are privately-owned parcels within the path of the Leon Creek Greenway currently designated as a "future trail" by the Howard W. Peak Greenway Trail System. The specific alignment has not yet been decided or funded for the continuation of the Leon Creek Greenway, and for this reason, City-owned land along Culebra Creek is currently designated as City/State/Federal Government. These properties, and other privately-owned properties throughout the area, are located within the floodplain. Perhaps there is a future opportunity for parks and open space in these flood-prone areas that cannot have commercial or residential developments.

Land Use Recommendations

Land use recommendations are identified to support the land use plan for the Highway 151 and Loop 1604 Area Regional Center. In the Implementation section of the plan, specific strategies are provided for each of the following recommendations.

Land Use Recommendation #1: Preserve and enhance the character and identity of established neighborhoods.

Land use patterns for the Highway 151 and Loop 1604 Area Regional Center preserve the character of established neighborhoods by providing exclusive low-density residential designations in appropriate areas; by concentrating higher intensity uses in designated areas and along major area roadways; and through the use of intermediate-intensity land uses as buffers and transitions between more active areas and residential areas.

Land Use Recommendation #2: Encourage efficient density patterns with commercial and mixed-use development at appropriate scales.

Mixed-use development is preferred in areas where higher density residential or commercial already exists, or in designated areas near major intersections or transit opportunities. Encourage higher

capacity commercial, business, and mixed-uses along highways and major arterials, while designating more local-serving uses to streets nearer to neighborhoods. High density residential development should be encouraged in this regional center. High density residential and vertical mixed-use projects are appropriate for areas with land use designations of Urban Mixed-Use, Regional Mixed-Use, and High Density Residential. In areas designated as Regional Mixed-Use, trip generation will likely be high, therefore transit-supportive uses and transit-oriented development will help to alleviate traffic problems.

Land Use Recommendation #3: Increase the amount of public park land, open spaces, and recreational areas.

While no land is currently allocated for parks and open space within the Highway 151 and Loop 1604 Area, there could be future opportunities for open space based on the complicated topography in the area and considerable floodplain coverage of vacant sites. There are plans to expand the Leon Creek Greenway through Culebra Creek, at the northern border of the plan area. Other similar natural assets that are difficult to build on could be utilized for open spaces, trails, or park land.

Future Land Use Categories

As described above, the Highway 151 and Loop 1604 Area Regional Center includes a range of land use designations that represent the unique character of the area, while encouraging and supporting development patterns that reflect the goals of the SA Tomorrow Comprehensive Plan and the preferences of the Highway 151 and Loop 1604 Area community stakeholders. Listed below is the full list of land use categories adopted by City Council into the Unified Development Code (UDC) on October 11, 2018. Each category listed includes a description, general guidance on where the land use designation is most appropriate, and a list of allowable zoning districts.

Residential Estate

Residential Estate includes large lot single-family detached houses on individual estate-sized lots or in conservation subdivisions. This form of development should be located away from major arterials, and can include certain nonresidential uses such as schools, places of worship, and parks that are centrally located for convenient neighborhood access. Permitted zoning districts: FR, R-20, RE, and RP.

Typical densities in this land use category would be up to 2 dwelling units per acre.

Low Density Residential

Low Density Residential includes single-family detached houses on individual lots, including manufactured and modular homes. This form of development should not typically be located adjacent to major arterials. This land use category can include certain nonresidential uses such as schools, places of worship, and parks that are centrally located for convenient neighborhood access. Permitted zoning districts: R-4, R-5, R-6, NP-8, NP-10, and NP-15.

Typical densities in this land use category would range from 3 to 12 dwelling units per acre.

IDZ and PUD may be considered consistent with this land use category, provided the permitted uses included on the zoning site plan and zoning ordinance are consistent with the uses and densities outlined above.

Urban Low Density Residential

Urban Low Density Residential includes a range of housing types including single-family attached and detached houses on individual lots, small lot residences, duplexes, triplexes, fourplexes, cottage homes, manufactured homes, low-rise garden-style apartments, and manufactured home parks. This land use category may also accommodate small scale retail and service uses that are intended to support the adjacent residential uses. Other nonresidential uses, including, but not limited to, schools, places of worship, and parks are appropriate within these areas and should be centrally located to provide easy accessibility. Permitted zoning districts: R-3, R-4, R-5, R-6, RM-5, RM-6, MF-18, MH, MHC, MHP, and NC.

Typical densities in this land use category would range from 7 to 18 dwelling units per acre.

IDZ, PUD, MXD, and TOD may be considered consistent with this land use category, provided the permitted uses included on the zoning site plan and zoning ordinance are consistent with the uses and densities outlined above.

Medium Density Residential

Medium Density Residential accommodates a range of housing types including single-family attached and detached houses on individual lots, manufactured and modular homes, duplexes, triplexes, fourplexes, and low-rise, garden-style apartments with more than four (4) dwelling units per building. Cottage homes and very small lot single-family houses are also appropriate within this land use category. Higher density multi-family uses, where practical, should be located in proximity to transit facilities. Certain nonresidential uses, including, but not limited to, schools, places of worship, and parks are appropriate within these areas and should be centrally located to provide easy accessibility. Permitted zoning districts: R-3, R-4, RM-4, RM-5, RM-6, MF-18, MF-25, MF-33, MH, MHC, and MHP.

Typical densities in this land use category would range from 13 to 33 dwelling units per acre.

IDZ, PUD, MXD, and TOD may be considered consistent with this land use category, provided the permitted uses included on the zoning site plan and zoning ordinance are consistent with the uses and densities outlined above.

High Density Residential

High Density Residential includes low-rise to mid-rise buildings with four (4) or more dwelling units in each. High density residential provides for compact development including apartments, condominiums, and assisted living facilities. This form of development is typically located along or near major arterials or collectors. High density multi-family uses should be located in close proximity to transit facilities. Certain nonresidential uses, including, but not limited to schools, places of worship, and parks are appropriate within these areas and should be centrally located to provide easy accessibility. This classification may be used as a transitional buffer between lower density residential uses and nonresidential uses. High density residential uses should be located in a manner that does not route traffic through lower-density residential uses. Permitted zoning districts: RM-4, MF-25, MF-33, MF-40, MF-50, MF-65, MH, MHC, and MHP.

Typical densities in this land use category would range from 25 to 50 dwelling units per acre.

IDZ, PUD, MXD, and TOD may be considered consistent with this land use category, provided the permitted uses included on the zoning site plan and zoning ordinance are consistent with the uses and densities outlined above.

Neighborhood Commercial

Neighborhood Commercial includes smaller intensity commercial uses such as small-scale retail or offices, professional services, and convenience retail and services that are intended to support the adjacent residential uses. Neighborhood commercial uses should be located within walking distance of neighborhood residential areas. Special consideration should be given to pedestrian and bicycle facilities that connect neighborhoods to commercial nodes. Permitted zoning districts: O-1, NC, and C-1.

IDZ, PUD, MXD, TOD, and MPCD may be considered consistent with this land use category, provided the permitted uses included on the zoning site plan and zoning ordinance are consistent with the uses and densities outlined above.

Community Commercial

Community Commercial includes offices, professional services, and retail uses that are accessible to bicyclists and pedestrians and linked to transit facilities. This form of development should be located in proximity to major intersections or where an existing commercial area has been established. Community commercial uses are intended to support multiple neighborhoods, have a larger market draw than neighborhood commercial uses, and attract patrons from the neighboring residential areas. All off-street parking and loading areas adjacent to residential uses should include landscape buffers, lighting and signage controls. Examples of community commercial uses include, but are not limited to, cafes, offices, restaurants, beauty parlors, neighborhood groceries or markets, shoe repair shops and medical clinics. Permitted zoning districts: O-1.5, NC, C-1, and C-2.

IDZ, PUD, MXD, TOD, and MPCD may be considered consistent with this land use category, provided the permitted uses included on the zoning site plan and zoning ordinance are consistent with the uses and densities outlined above.

Regional Commercial

Regional Commercial includes high intensity uses that draw customers from both adjacent communities as well as the larger metropolitan region. Regional commercial uses are typically located in general proximity to nodes along expressways or major arterial roadways and incorporate high-capacity transit facilities. Regional Commercial uses should incorporate well-defined entrances, shared internal circulation, limited curb cuts to expressways and arterial streets, sidewalks and shade trees in parking lots, landscaping between the parking lots and roadways, and well- designed monument signage. Examples of regional commercial uses include, but are not limited to, movie theaters, plant nurseries, automotive repair shops, fitness centers, home improvement centers, hotels and motels, mid- to high-rise office buildings, and automobile dealerships. Permitted zoning districts: O-1.5, O-2, C- 2, C-3, L, and BP.

IDZ, PUD, MXD, TOD and MPCD may be considered consistent with this land use category, provided the permitted uses included on the zoning site plan and zoning ordinance are consistent with the uses and densities outlined above.

Neighborhood Mixed-Use

Neighborhood Mixed-Use contains a mix of residential, commercial, and institutional uses at a neighborhood scale. Within mixed-use buildings, residential units located above first floor are encouraged. Typical first floor uses include, but are not limited to, small office spaces, professional services, and small-scale retail establishments and restaurants. The mix of uses may be vertically or

horizontally distributed, and there is no requirement that a single building contain more than one use. Live/work housing options are permissible in Neighborhood Mixed-Use area to ensure access to housing options and services within close proximity for the local workforce. Where practical, buildings are situated close to the public right-of-way, and parking is located behind buildings. Parking requirements may be minimized using a variety of creative methods, such as shared or cooperative parking agreements, to maximize land available for housing and community services. Pedestrian spaces are encouraged to include lighting and signage, and streetscaping should be scaled for pedestrians, cyclists, and vehicles. Properties classified as Neighborhood Mixed-Use should be located in close proximity to transit facilities. Permitted zoning districts: RM-4, RM-5, RM-6, MF-18, O-1, NC, C-1, MH, MHC, MHP, FBZD, AE-1, and AE-2.

IDZ, PUD, MXD, TOD and MPCD may be considered consistent with this land use category, provided the permitted uses included on the zoning site plan and zoning ordinance are consistent with the uses and densities outlined above.

Urban Mixed-Use

Urban Mixed-Use contains a mix of residential, commercial, and institutional uses at a medium level of intensity. Urban Mixed-Use development is typically larger-scale than Neighborhood Mixed-Use and smaller-scale than Regional Mixed-Use, although many of the allowable uses could be the same in all three categories. Building footprints may be block-scale, but could be smaller depending on block configuration and overall development density. Typical first floor uses include, but are not limited to, professional services, offices, institutional uses, restaurants, and retail including grocery stores. The mix of uses may be vertically or horizontally distributed, and there is no requirement that a single building contain more than one use. Live/work housing options are permissible in Urban Mixed-Use areas to ensure access to housing options and services within close proximity for the local workforce. Structured parking is encouraged in Urban Mixed-Use category, but is not required. Parking requirements may be satisfied through shared or cooperative parking agreements, which could include off-site garages or lots. The Urban Mixed-Use category should be located in proximity to transit facilities. Permitted zoning districts: RM-4, RM-5, RM-6, MF-18, MF-25, MF-33, MF-40, O-1, O-1.5, C-1, C-2, MH, MHP, MHC, FBZD, AE-1, AE-2, AE-3, and AE-4.

IDZ, PUD, MXD, TOD and MPCD may be considered consistent with this land use category, provided the permitted uses included on the zoning site plan and zoning ordinance are consistent with the uses and densities outlined above.

Regional Mixed-Use

Regional Mixed-Use contains residential, commercial and institutional uses at high densities. Regional Mixed-Use developments are typically located within regional centers and in close proximity to transit facilities, where mid-rise to high-rise buildings would be appropriate. Typical lower floor uses include, but are not limited to, offices, professional services, institutional uses, restaurants, and retail including grocery stores. The mix of uses may be vertically or horizontally distributed, and there is no requirement that a single building contain more than one use. Live/work housing options are permissible in Regional Mixed-Use areas to ensure access to housing options and services within close proximity for the local workforce. Where feasible, development is ideally built at the block scale, with minimum building setbacks. Parking requirements may be satisfied through shared or cooperative parking agreements, which can include off-site garages or lots. If parking requirements are satisfied on-site, structured parking is encouraged. Pedestrian spaces are encouraged to be generous in width and lighting, with streetscaping and signage scaled to pedestrians. Regional Mixed-Use projects encourage incorporation

of transit facilities into development. Permitted zoning districts: MF-33, MF-40, MF-50, MF-65, O-1.5, O-2, C-2, C-3, D, ED, FBZD, AE-1, AE-2, AE-3, and AE-4.

IDZ, PUD, MXD, TOD and MPCD may be considered consistent with this land use category, provided the permitted uses included on the zoning site plan and zoning ordinance are consistent with the uses and densities outlined above.

Employment/Flex Mixed-Use

Employment/Flex Mixed-Use provides a flexible live/work environment with an urban mix of residential and light service industrial uses. Uses include smaller-scale office, retail, art studio warehouses, art-oriented fabrication, creative businesses and work spaces, and cottage industrial and fabrication uses. Adaptive uses of vacant or underutilized structures are encouraged to provide residential urban infill and appropriate employment opportunities within or in close proximity to neighborhoods. Buildings have a smaller footprint and can closely resemble campus-like development across multiple sites or with several multi-functioning buildings on one site. Permitted zoning districts: RM-4, MF-18, MF-25, MF-33, O-1, O-1.5, C-1, C-2, L, AE-1, AE-2, AE-3, and AE-4.

IDZ, PUD, MXD, TOD and MPCD may be considered consistent with this land use category, provided the permitted uses included on the zoning site plan and zoning ordinance are consistent with the uses and densities outlined above.

Business/Innovation Mixed-Use

Business/Innovation Mixed-Use accommodates industrial uses with office, commercial, and residential uses, all within a cohesive setting, on a larger scale and within larger footprints than the Employment/Flex Mixed-Use category. Industrial arts workshops, high tech fabrication, processing and assembly, and other industrial uses are permitted, in addition to commercial uses. Vocational training, technological learning centers, medical campuses, and research/development institutions are also appropriate for these spaces. Additional environmental performance standards should be employed for properties designated as Business/Innovation Mixed-Use, such as hours of activity, loading, noise levels and lighting, to ensure that the intensity of the industrially oriented uses is comparable to that of the other non-residential uses. The mix of uses may be either vertically or horizontally distributed. Live/work housing options are permissible in Business/Innovation Mixed Use areas to ensure access to housing options and services within close proximity of business innovation areas for the local-workforce. Business/Innovation mixed-use should incorporate transit and bicycle facilities to serve the training and employment base. Permitted zoning districts: RM-4, MF-18, MF-25, O-1.5, O-2, C-2, C-3, L, I-1, MI-1, BP, AE-1, AE-2, AE-3, and AE-4.

IDZ, PUD, MXD, TOD and MPCD may be considered consistent with this land use category, provided the permitted uses included on the zoning site plan and zoning ordinance are consistent with the uses and densities outlined above.

Light Industrial

Light Industrial includes a mix of manufacturing uses, business park, and limited retail/service uses that serve the industrial uses. Industrial uses should be screened and buffered from adjoining non-industrial uses. Any outside storage should be under a roof and screened from public view. Examples of light industrial uses include drug laboratories, furniture wholesalers, lumberyards, food production, and warehousing. Permitted zoning districts: L, I-1, MI-1, and BP.

IDZ, TOD, and MPCD may be considered consistent with this land use category, provided the permitted uses included on the zoning site plan and zoning ordinance are consistent with the uses and densities outlined above.

Heavy Industrial

Heavy Industrial includes heavy manufacturing, processing and fabricating businesses. Heavy industrial uses shall be concentrated at arterials, expressways, and railroad lines. This category is not compatible with neighborhood-scaled categories or those that permit residential zoning. Heavy Industrial should be separated from non-industrial uses by an allowable land use or a significant buffer. Examples of heavy industrial uses include auto manufacturing, battery manufacturing, and petrochemical bulk storage. Permitted zoning districts: I-1, I-2, MI-1, MI-2, QD, and SGD.

Agricultural

Agricultural includes crop agriculture, ranching, and related agribusiness practices. Single-family detached houses and detached accessory dwelling units are permitted on agricultural and ranch lands at very low densities or in conservation subdivisions that will not interfere with agricultural operations. Limited commercial uses directly serving agricultural and ranching uses, such as farmers markets, nurseries, stables, bed and breakfasts are permitted. To maintain scenic qualities, natural vegetative buffers, deeper setbacks, increased signage control, earthen drainage channels, and more restrictive access management standards are desired along major scenic corridors. Floodplain protection and buffer zones along creeks and rivers are instrumental in retaining rural character. Permitted zoning districts: RP and FR.

Parks/Open Space

Parks/Open Space may include, but is not limited to, large, linear, or unimproved land where conservation is promoted, and development is not encouraged due to the presence of topographic constraints or institutional uses on the site. Parks/Open Space may include utility corridors and public or private land uses that encourage outdoor passive or active recreation. Examples include city owned and/or operated pocket, regional, or linear parks, as well as private parks associated with subdivisions and neighborhood associations.

City/State/Federal Government

City/State/Federal Government includes areas owned and operated by a federal, state, or city agency. Examples may include government offices, public service facilities such as libraries and police stations, military bases, state colleges, and federal courts. This category does not apply to properties owned by a public agency but leased to and operated by another party.

Focus Areas

[See **Figure 5: Focus Area Framework Map**]

The focus areas and corridors within the Highway 151 and Loop 1604 Area Regional Center were identified by the Planning Team and the community as areas that have capacity for growth and change and can become examples of the types of development the area would like to see in the future. While each area is unique, they all present opportunities to accommodate population and employment growth while simultaneously fulfilling the SA Tomorrow goal of creating compact walkable places where San Antonio residents can live, work, and play.

As the Highway 151 and Loop 1604 Area continues to grow, there will be growing need for more places that attract residents, workers, students, and visitors to the area. The Planning Team members expressed a desire to see more mixed-use in targeted locations within the Plan Area, and the focus areas are potential sites for this type of development. The focus areas are envisioned as ideally mixed-use developments that incorporate parks, plazas, and multimodal streetscapes. The focus areas identified within the Highway 151 and Loop 1604 Area Regional Center are all vacant parcels. This means that there are considerable opportunities for investment and new development projects. As new developments occur, special consideration should be given to those that increase the amount of publicly accessible space within the area. For the Highway 151 and Loop 1604 Area Regional Center in particular, there are many large parcels with areas of floodplain or other suitable land where developers could be encouraged to dedicate land toward park space.

Each focus area has an individually tailored vision created through conversations with the Planning Team members. The focus area visions are rooted in the context of existing land uses, proposed land uses, and the desired character and design of the areas. While some focus areas have more of a commercial focus, others are more residential, and some have more retail or recreation options. Identifying the purpose, future character, appropriate building heights, and areas of transition within each focus area creates individualized areas that are unique to the regional center.

While these ideas for the focus areas are not necessarily going to be developed exactly as they are imagined, this exercise is intended to provide a more detailed picture for the desired types of growth within the plan area as it aligns with the future land use map. While recommendations for the regional center plan are generally broad in terms of policy, the focus area exercise allows for more detailed conversations around building form, streetscape, and other aspects of urban design and connectivity. In the rest of this section, the focus areas are described in detail with their respective visions and potential growth opportunities, as discussed by the Planning Team and other stakeholders. In order to realize these ideas, a set of recommendations are also provided to encourage the type of growth desired for the regional center and its focus areas.

Focus Area #1: West Highway 151

[See **Figure 6: Focus Area #1 Map – West Highway 151**]

West of Highway 151 and south of Wiseman Boulevard lies a considerably large area of mostly vacant land. This site is near Northwest Vista College, the CHRISTUS Santa Rosa Hospital – Westover Hills, and the GM Financial office. Westover Link and Slick Ranch Creek run through the focus area, providing alternative access between Highway 151 and North Ellison Drive. Although it is a natural amenity and

asset for the focus area, the presence of the creek creates flooding concerns. This favorable topography alleviates major flooding issues in other parts of the focus area.

Vision

The vision for the West Highway 151 Focus Area is to develop the majority of the area into affordable housing options for Northwest Vista College students, recent graduates, or employees of the neighboring businesses. In addition to housing, there is a strong desire to provide a key transit connection and associated amenities that could serve the focus area, regional center, and surrounding area. The transit connection and associated amenities will also serve retail or restaurant options for students, residents, or those waiting for transportation. Proposed public art and wayfinding along the borders of the focus area ensure safety and provide character for the mixed-use vision.

To better connect the site to the surrounding area, proposed Priority Connections between the Northwest Vista College entrance and Westover Link, as well as to Highway 151 from Wiseman Boulevard have been identified and are shown on Focus Area #1 Map. Along Slick Ranch Creek and the floodplain area, there is strong potential for an internal trail connection, which would both alleviate drainage and impervious cover concerns and provide recreational opportunities in this regional center that is in short supply of green spaces. Working in combination with the proposed priority connections, the internal trail connection ensures the walkability of the mixed-use development and avoids creating superblocks. To create an enjoyable street experience, streetscape improvements along Wiseman Boulevard would provide walkability along the roadway as well as a safe direct connection between Northwest Vista College and the mixed-use development.

[See **Figure 7: Focus Area #1 – Existing**]

[See **Figure 8: Focus Area #1 – Conceptual Rendering**]

Transformative Project

Serving as the largest focus area identified within the regional center and with the largest amount of vacant land, this focus area was evaluated in more detail in a digital design charrette exercise conducted with members of the Planning Team. This exercise allowed participants to imagine what their vision for the focus area would look like with an example rendering of buildings and other concepts. A view of the conceptual design is shown in Focus Area #1 – Conceptual Rendering, with the view of this rendering oriented southeast from the perspective of the Northwest Vista College. This image displays the desired mixed-use design that allows walkability within the development and to surrounding uses, such as the college campus. It also highlights the density envisioned for the development, which included a variety of housing types blended with first floor activated commercial and retail spaces. The goal was to create a walkable node where one could park and visit multiple destinations by foot.

To mitigate potential flooding issues, all structures and the majority of the impervious coverage in the area are situated outside of the floodplain. The Planning Team imagines the floodplain as a recreational asset and as an opportunity to provide potential community park and ballpark space. This provides an area that supports the future residents in the mixed-use development, the community as a whole, and encourages visitors, students, and workers to extend their stay and explore.

Focus Area #2: East Highway 151

[See **Figure 9: Focus Area #2 Map – East Highway 151**]

On the east side of Highway 151 lies another considerably large area of vacant land that is located south of Wiseman Boulevard and bordered by Rogers Road to the east. Bordering the site across Rogers Road are some hotel developments and major employment centers, including One Frost, Kohl's Operations Center, Chase, and Aetna, among others.

Vision

The vision for the East Highway 151 Focus Area is more of a hospitality hub, including restaurant options for nearby employees to frequent during their lunch breaks, due to the proximity to existing hotels and employment centers. Instead of a mixed-use area with residences and retail, this focus area is envisioned as more of a mix of hospitality, restaurant, and open space uses.

Similar to the West Highway 151 Focus Area, this site does not have many options for internal circulation. Creating an internal network of streets that provide a central connection from Wiseman Boulevard to Town Center Drive and an east-west connection from Rogers Road to the Highway 151 access road supports the mixed-use and pedestrian friendly environment vision of the focus area while creating a large scale grid-like pattern of development. The streetscape improvements, signage, and wayfinding proposed along Rogers Road would provide a safe and comfortable environment for residents and workers at nearby campus headquarters, such as the employees at One Frost, to walk or bike to the focus area. A proposed plaza and proposed trail extension along Slick Ranch Creek would be community-wide destinations. Public art at the plaza and the signage and wayfinding could add character definition to the focus area.

[See **Figure 10: Focus Area #2 – Existing**]

[See **Figure 11: Focus Area #2 – Conceptual Rendering**]

Transformative Project

Similar to the West Highway 151 Focus Area, the vision of the East Highway 151 Focus Area was brought to life in visualizations created in response to the priorities of the Planning Team through the digital design charrette process. The proposed hospitality, retail, and dining development is shown in the Focus Area #2 – Conceptual Rendering, with office, open space, and restaurant uses. Although there are a variety of uses incorporated, the development appropriately reserves most of its square footage for office uses. This is compatible with the several corporate campuses located on the eastern side of Highway 151, and could accommodate smaller or larger commercial businesses as needed. Multi-family residential uses could also be incorporated to allow for more street life, and perhaps could be integrated into the hotel or office developments as appropriate.

The development is not envisioned to be insular and is designed to be accessible by foot, vehicle, or bike for residents and employees located east of Rogers Road. Plazas, promenades, and open spaces are key elements in the vision for this site, and would provide outdoor recreational and leisure opportunities to lunch-time visitors, community residents, or guests at the nearby hotels. Open space should also contain street furniture, shading, and other elements to encourage people to stay a while in all weather. Open space is located on the southern portion of the focus area. It serves as a refuge from the activity of the regional center and mixed-use development allowing users to interact with Slick Ranch Creek.

Focus Area #3: West Loop 1604

[See **Figure 12: Focus Area #3 Map – West Loop 1604**]

This long and narrow focus area lies west of Loop 1604, with Wiseman Boulevard running through the northern part of the site and Caracol Creek creating floodplain concerns to the south. The site is predominantly vacant, although there are development plans currently underway on the southern portion of the focus area that support its overall vision. Considerable portions of the parcels in this focus area extend outside of the city limits.

[See ~~Figure 13: Focus Area #3—Existing and Proposed Land Use/Massing~~]

Vision

The West Loop 1604 Focus Area is envisioned as more of a commercial [employment, and workforce](#) hub given the area's proximity to Loop 1604. ~~Focus Area #3—Existing and Proposed Land Use/Massing presents a land use and massing conceptual design for the focus area.~~ Integration of priority connections and an internal trail network along Caracol Creek provide outdoor recreational opportunities and support interconnectivity goals. Signage and wayfinding and public art identifiers for this development are crucial for safe vehicular movement into and out of the development, especially along a fast-moving corridor such as the Loop 1604 frontage road.

Safety priorities are reinforced in the north portion of the focus area by integrating streetscape improvements along Summer Breeze Lane and the Loop 1604 frontage road. The streetscape improvements make pedestrian and bicyclist activity safer amongst vehicular traffic by adding more street trees, wider sidewalks, and defined crosswalks. These improvements permit residents and visitors to safely access the proposed priority redevelopment nodes adjacent to the Loop 1604 frontage road, located north and south of Wiseman Boulevard.

Focus Area #4: Wiseman Boulevard Mixed-Use Corridor

[See **Figure 1314: Focus Area #4 Map – Wiseman Boulevard Mixed-Use Corridor**]

Wiseman Boulevard is one of the major connectors of the residential and commercial uses within the Highway 151 and Loop 1604 Area Regional Center. This street travels along Northwest Vista College, past Focus Area #1 and Focus Area #2, and connects into Rogers Road. The western edge of the regional center is the intersection of Wiseman Boulevard and Loop 1604 and is bound on the east right past the Wiseman Boulevard and Rogers Road intersection. Focus Area #4 includes Wiseman Boulevard from Loop 1604 and Rogers Road. This mixed-use corridor is located at the heart of the regional center and is a vital component to existing and future economic development opportunities.

[See **Figure 1415: Focus Area #4 – Conceptual Rendering**]

Vision

Since there is considerable vacant land on either side of the Wiseman Boulevard corridor, there are major opportunities as development occurs to create a safer pedestrian, bicyclist, and transit environment along the thoroughfare. This would be especially important if there were to be transit investment within Focus Area #1. The corridor should provide safer crossings, so students from Northwest Vista College can easily walk to the mixed-use area within Focus Area #1, utilize recreational amenities, and perhaps utilize available transit options to their next destination. Improved crossings are the major need for the corridor, but also a more balanced and multimodal approach to the street traffic, which can often travel at fast speeds. An example of an improved crossing at Northwest Vista College's

entrance off Wiseman Boulevard is shown in the Focus Area #4 – Conceptual Rendering. These crossing improvements create an atmosphere that still accommodates all modes of transit while prioritizing the safety of pedestrians by including wide and well-marked crosswalks, landscaped traffic islands, and ADA compliant sidewalks and ramps.

Focus Area #5: Rogers Road Mixed-Use Corridor

[See Figure 1546: Focus Area #5 Map – Rogers Road Mixed-Use Corridor]

Rogers Road is another major connector for the plan area. Residents of the neighborhoods on either side of the corridor describe it as a “pass through” for many motorists who are wanting to avoid traffic on either Culebra Road or Highway 151. The northern end of this focus corridor is the Rogers Road and Culebra Road intersection, with the southern end at Rogers Road and Wiseman Boulevard. This corridor has an almost rural character in areas, especially along the Mountain View Acres neighborhood. However, with a new Methodist Hospital planned for the vacant parcel behind the Culebra Market development, this corridor will need to support many other types of users in the future.

[See Figure 1647: Focus Area #5 – Conceptual Rendering]

Vision

This corridor is envisioned to expand to become more mixed-use and will need to support more modes of travel. Central to this will be better pedestrian crossings, in addition to safer travel for those who might be bicycling or taking transit. The Conceptual Rendering of Focus Area #5 shows a potential rendering for how more mixed-use activity or commercial activity could front the street, maintaining adequate parking while screening with trees and creating a desirable walking environment. The rendering also represents a vision that provides opportunity for sufficient parking necessary for large campus and mixed-use developments while providing safe access and usability for pedestrians. This concept supports the current nature of the corridor while providing opportunity for safety measures to be integrated.

Focus Areas Recommendations

Focus Areas recommendations were developed based on the Highway 151 and Loop 1604 Area’s Vision and Goals and to address the challenges identified during the planning process. Specific strategies to implement these recommendations can be found in the Implementation section of the plan.

Focus Area Recommendation #1: Encourage the creation of park, open, recreation, or civic space in the identified focus areas.

Throughout the process for this regional center plan, residents and workers all identified a lack of publicly accessible open space as a key challenge for the area. Several major landowners, such as Northwest Vista College, GM Financial, and Capital Group, have created open space within their developments on existing floodplain, with varying degrees of public access. As the area grows, publicly accessible park, open, recreation, and civic spaces will be vital for visitors, residents, and workers. These types of spaces could be created through public plazas surrounded by mixed-use development, through official park spaces, or through dedicated community centers.

Focus Area Recommendation #2: Protect the established character of the regional center in each focus area by providing landscaped buffers and screening, and encouraging low impact development.

Residents of the regional center consistently mention the “natural” or “Hill Country” character of the area when describing what they like and what they would like to preserve. Some of this character is due to the topography of the landscape and existing trees, but much of it is due to regulations in place to maintain certain requirements for development. In order to protect this established character certain regulations, such as the expansion or creation of overlay zoning districts and the encouragement of Low Impact Development (LID), should be explored for the focus areas.

Focus Area Recommendation #3: Support the unique vision for each focus area and emerging industries within the regional center by creating high-quality places with a mix of uses, vibrant public spaces, transit accessibility, and appropriate transitions to surrounding uses.

Planning Team members had detailed discussions about the types of developments that would be desirable for the focus areas, and how these types of uses could support the emerging healthcare, hospitality, and financial services industries increasingly locating in the area. Of course the specifics of new developments are up to the individual developers and property owners, but zoning and land use can guide the types of developments desired. For this reason, zoning and development policies in the area should be evaluated to ensure they support and promote the community’s desired vision for the area. Transportation connectivity is also essential, and requires continued coordination with VIA Metropolitan Transit.

Mobility

[See Figure 1748: Mobility Framework Map]

Background and Vision

In 2016, the City of San Antonio adopted the [SA Tomorrow Multimodal Transportation Plan](#), to make the city's transportation system "sustainable, safe, convenient, efficient, and inclusive of all modes." The plan adopted by City Council established "a shift in focus from moving vehicles to moving people" with the goal of managing traffic congestion while improving transportation choices. The plan identified two primary and interdependent methods for managing future traffic congestion:

- Develop land use patterns and policies to promote local trips.
- Encourage more transportation options, beyond personal vehicles, to connect regional centers.

A city's land use pattern is inextricably linked to its mobility options and traffic concerns. [The SA Tomorrow Multimodal Transportation Plan](#) acknowledged that San Antonio cannot build its way out of congestion by continuously adding additional lanes to major highways. Rather, it must take a holistic approach to improve mobility by implementing the Comprehensive Plan through future land use planning. By welcoming more people to live, work, and play in urban and regional centers, and along transit corridors, the city can offer shortened trip lengths, more transportation choices, and an improved quality of life.

The combined cost of housing and transportation (commonly referred to as H+T) is often a large portion of a household budget. In terms of affordability, experts recommend that a household spend no more than 45% of their income on H+T. However, the average household in the Greater San Antonio Region spends more than this recommended share, with 53% of an average household income going toward housing and transportation costs. Walkable communities that provide great transit options can reduce transportation costs for the average household. If people have an alternative to driving alone, transportation costs are generally stable even when gas prices rise. As more people choose to reach their destination on foot, bicycle, or transit, the number of cars on the road will be reduced. This will in turn, minimize traffic delays for those who choose to drive.

Experiencing continual growth for the last several decades, the Highway 151 and Loop 1604 Area Regional Center has quickly transformed from ranchland and very few residential developments into a center for recreation, economic activity, and many housing developments. Major road infrastructure, such as Highway 151 and connections to U.S. Highway 90 and Loop 1604, supported major growth in tourism in the 1990s, with the arrival of SeaWorld San Antonio and the Hyatt Regency Hill Country Resort and Spa. Today, the regional center has emerged as a major employment node with a diverse employment base anchored by Wells Fargo, Capital Group, Frost Bank, and other large data companies including GM Financial, Microsoft, and QVC. In addition to ample employment opportunities, this area is home to Northwest Vista College, which provides higher education for thousands of students each year.

The Highway 151 and Loop 1604 Area Regional Center is bisected by Highway 151 and bordered on the west by Loop 1604. These major highways provide access to the area's employment opportunities, but also serve as barriers to those traveling between destinations within the regional center. Highway interchanges and traditional suburban land use patterns cause traffic congestion issues throughout the community. The vision for this regional center is to provide safe and efficient transportation among all

modes of travel by creating more transit supportive land uses and encouraging transportation options that will alleviate future pressures on transportation corridors while supporting growth.

Highway 151 and Loop 1604 Area Regional Center's Mobility Needs

Transportation investments must be continuously made to move large volumes of freight efficiently, to link neighborhood developments to growing employment opportunities, and to provide adequate transit routes that connect to regions outside of the plan area. Growing populations demand streets that not only carry people, goods, and services, but that also serve as playgrounds, parks, and public spaces. The key mobility needs identified in this regional center include:

- Balancing mobility between many mixed-use corridors and high functioning arterials while maintaining “natural” character;
- Improving neighborhood connectivity and infrastructure quality;
- Supporting high capacity transit as a catalyst for economic development; and
- Connecting neighborhoods to trail infrastructure and parks by taking advantage of existing rights-of-way and identifying opportunities for pedestrian and bicycle infrastructure.

A set of high-level street classification recommendations were developed to guide decision-making and address the transportation needs in the Highway 151 and Loop 1604 Area Regional Center.

Recommendations shown in the Mobility Framework Map include street priorities for travel modes (i.e., automobiles, freight, transit/pedestrians), intersections identified for enhancements, and mobility hubs proposed for transit service. Street type and use recommendations are shown in the Street Types Map. The mobility recommendations listed below will be further refined in a coordinated manner with the City's Transportation and Public Works Departments and other relevant partners such as TxDOT, VIA, and the AAMPO.

Highway 151 and Loop 1604 Area Regional Center Mobility Framework

The Mobility Framework Map lays out the vision for accommodating the expanding mobility needs of the Highway 151 and Loop 1604 Area Regional Center. Principle considerations to ensure a mobility network that is safe and inviting for people who walk, shop, park, and drive in the regional center, include:

Modal Priority Corridors

Streets play a multifaceted role in the plan area, serving as arteries for traffic and as public spaces for walking or cycling. Changes to street design can support the future land use plan and the vision for the regional center. The modal priority corridors are differentiated in varying colors in the Mobility Framework Map. Modal priority reflects the degree to which motorists, bicyclists, transit users, and pedestrians will interact with and utilize the corridor. Modal priority corridors guide design treatments to benefit a particular mode, but do not exclude other users. Establishing priority for pedestrians or transit over cars or bikes, for example, can help guide tradeoff decisions during the design of a specific street.

Automobile

Automobile corridors are envisioned to support high levels of daily motor vehicle travel. These corridors - interstates, highways, arterials, and primary roadways - provide connections to major destinations and link the regional center to other plan areas. Significant automobile corridors in the Highway 151 and Loop 1604 Area Regional Center include Culebra Road, Westover Hills Boulevard, and West Military

Drive, all of which traverse east-to-west and have been identified to enhance mobility within the regional center and to other parts of the city. Automobile corridor improvements can relieve traffic congestion and enhance roadway design, to increase the safety of motor vehicle travel.

Freight

Freight corridors are envisioned to support the movement of high levels of goods by freight trucks that travel between industrial or commercial areas and major destinations. Priority freight corridors in the plan area include Loop 1604, Highway 151, and Potranco Road.

Bicycles

Bicycle corridors are envisioned to support biking as a convenient, safe, and comfortable option for all ages and abilities. An integrated network of bicycle routes and well-designed streets in key activity areas can fill the gaps in the multimodal system this plan hopes to support. The mobility framework focuses on completing the bicycle network and recommends new connections to directly access trails and other amenities. The two priority bicycle corridors in the Highway 151 and Loop 1604 Area Regional Center are along 1) Copperfield Road/Rousseau Street—to provide safe bicycle connections from residential areas to locations such as the San Antonio Blue Ramp Skatepark, SeaWorld, the Trophy Ridge Recreation Center, and Nautilus Amphitheater; and 2) Impala Summit—to provide access to the Westover Valley and Stonegate Hill neighborhoods, with the possibility for future access to the extension of the Leon Creek Greenway through Culebra Creek behind these neighborhoods.

Multi-Use Trail

Multi-use trail corridors are envisioned as continuous, separated arteries that provide combined pedestrian/bicycle paths that connect numerous destinations such as parks, shops, schools, libraries, and residential areas. A multi-use trail provides access points to regional amenities and improves connections for people who walk, stroll, or bicycle. Within this regional center, there is potential to develop multi-use trail pathways that provide east-west connections along Culebra Creek, West Military Drive, Westover Hills Boulevard, and Wiseman Boulevard. There is an additional opportunity to provide a north-south connection between Easterling and Impala Summit, which would connect to Culebra Creek through the Westover Valley neighborhood.

Balanced/Multimodal

Balanced and multimodal corridors are envisioned as “complete streets,” a type of street design that considers all users and accommodates persons of all ages and abilities, whether children, older adults, or persons with disabilities. These corridors balance the priorities of multiple modes of travel and require continual community conversations and case-by-case design to best align with the community vision. The function of the road, level of traffic by mode, and adjacent land use and intensity will help determine the road type and design features. As the Highway 151 and Loop 1604 Area Regional Center continues to grow in terms of employees, residents, and commercial services, mobility needs can be addressed by developing multimodal and balanced transportation access throughout the entire plan area. Ellison Drive and Rodgers Road have been identified as two priority corridors that bisect the regional center and provide an opportunity to enhance mobility options for motorists, bicyclists, and pedestrians.

Pedestrian Focus Areas

Pedestrian focus areas refer to specific nodes of pedestrian activity that provide adequate and safe pedestrian infrastructure connecting residential areas to amenities such as transit routes, commercial developments, parks, schools, and restaurants. These areas should have large sidewalks, safe

intersections, ample crossing opportunities, and other design characteristics focused on keeping pedestrians safe. Community members identified pedestrian priority locations near 1) major destinations, including the intersection of Military Drive and Ellison Drive, 2) areas with ample amenities and destinations, including the One Frost/Kohls business park, and 3) major neighborhood origins, such as along Culebra Road.

Intersection / Crossing Enhancements

Intersection and crossing enhancements are envisioned as intuitive and predictable crossings for all users. Enhancement locations are identified where crossing is considered a priority, where complex movements occur, and when potential conflicts are likely between pedestrians, motorists, and bicyclists, all who share space. Whether signalized or un-signalized, crossings where a high level of multimodal activity is considered and planned for will facilitate predictability for all users. Intersection enhancements can include the lowering of vehicular travel speeds and ensuring high pedestrian visibility. The priority intersection and crossing enhancement locations are denoted on the Mobility Framework Map with orange squares.

Mobility Hubs

Mobility hubs are envisioned as scalable nodes that host mobility options, such as frequent transit, shared rides, bicycling, and micro-mobility. These areas link multimodal access and connectivity with amenities such as adequate lighting, shelters, benches, real-time information, accessible sidewalks, and pedestrian crossings. VIA has plans to develop a new Park and Ride just outside the Highway 151 and Loop 1604 Area Regional Center boundary (noted on the map as a mobility hub). Identified in VIA's long-range plan, this transit facility is planned to be in the Alamo Ranch area to serve express transit and potential mobility-on-demand services. Four smaller mobility hubs are identified along West Military Drive, Westover Hills Boulevard, Wiseman Boulevard, and at Loop 1604 and Culebra Road.

Highway 151 and Loop 1604 Area Regional Center Street Types

As communities evolve and grow, so do the demands on the mobility system. The location and type of growth in an area or along a corridor help determine the demand on the transportation network and the viability of various transportation options. This interdependence makes it crucial to plan for transportation and land use collectively. Streets are organized not only by role and function but also by their character and surrounding context.

Future land use designations that encourage a mix of uses and higher density developments located on major roadways will lead to additional pedestrians, bicyclists, transit riders, cars, and freight delivery trucks of various sizes. A future land use plan also influences other factors, such as drop-off delivery zones, shared mobility hubs, and walkability. The Street Types Map identifies the interactions between future land use and transportation plans. The street type classifications integrate transportation and land use to guide context-sensitive mobility solutions.

Creating Street Types

To conceptualize the types of streets needed to align with future land use designations, several Street Types were developed. Streets are categorized by "functional classification" by the City of San Antonio's Major Thoroughfare Plan. This functional classification describes the intensity of vehicular traffic on the roadways. For example, a classification of Super Arterial has the primary job of moving cars quickly to further destinations than a local/other classification, which would have slower vehicle speeds and lighter traffic with a more pedestrian-friendly environment.

[See **Figure 1849: Street Types: Functional Classification and Land Use Context Matrix**]

To create the Street Types, these functional classifications are intersected with the land use context. The Functional Classifications are shown in the left column of the matrix, and the Land Use Context is shown in the top row. The cells of this table show the Street Types, which are shaded in a gradient of colors – with a lighter blue signifying a priority for moving vehicles, and a darker blue signaling a focus on people and places. As an example, a local road in a mixed-use context would be very walkable, and allow people to walk or bike between shops or residences. On the contrary, some roads are necessary for moving vehicles and freight, which could be examples of Super Arterials, Primary Arterials, or Rural roads in Suburban Commercial, Industrial, or Mixed-Use areas.

Not all streets can be fully for cars because the future for San Antonio is more walkable and includes more transportation options. Contrastingly, not all streets can be fully for pedestrians, because there is still a need to move goods, provide fast and efficient transit, and to accommodate people traveling longer distances. This Street Type exercise allows us to see which streets should be prioritized for future changes to make them consistent with the future land use map, and to serve the vision for the regional center and the city overall.

Street Types in the Highway 151 and Loop 1604 Area Regional Center

This combination of functional classification and land use context created a menu of Street Types that was then applied to the Highway 151 and Loop 1604 Area Regional Center. As shown in the Street Types Map, the intersection of the functional classification and land use context are shown in varying patterns and colors on each of the major roadways in the regional center.

[See **Figure 1920: Street Types Map**]

The major highways of Highway 151 and Loop 1604 are classified as major freeways by the Major Thoroughfare Plan – rapidly moving goods and traffic with major commercial uses on either side of the highway, and perhaps some mixed-use in the future.

Wiseman Boulevard, Rogers Road, Westover Hills Boulevard, and Culebra Road are classified as Secondary Arterials in a Mixed-Use Residential/Storefront Context. These streets still need to move goods and traffic, as they are some of the main inner roads of the regional center, but in the future the surrounding context of these roads is set to become more mixed-use with a residential and retail focus. This means the streets should accommodate more modes of travel such as pedestrians, bicyclists, and transit to attract and accommodate more residents and visitors to the mixed-use destinations along these roadways.

Other parts of the regional center are envisioned currently, and in the future, as employment centers. The roadways supporting these uses, such as West Military Drive, North Ellison Drive, and the southern portion of Rogers Road below Westover Hills Boulevard, are classified as Secondary Arterials but in a Mixed-Use/Employment context. These roadways still need to move more goods, but can also serve the employees who work in these areas. There might be fewer surrounding residential and retail uses, but perhaps the employees who work in these areas would like sidewalks, transit, and bicycle infrastructure to better access other parts of the regional center.

To more simply show the vision for the Street Types in the regional center, the Streets for People and Places Map shows the streets classified into the same gradient of intensity between car-focused and people/places-focused streets. As shown on the map, most of the roadways in the regional center should have a more people and places-focused approach going forward to support the future land use vision. Major, auto-focused thoroughfares will still be needed, but many smaller roads in mixed-use contexts should provide more modes of travel available to residents and employees.

[See [Figure 2021: Streets for People and Places Map](#)]

Relationship between Street and Building

Safe and vibrant streetscapes are influenced by the way in which buildings relate to and integrate with the sidewalk realm, and differing levels of this interaction will be present in the regional center. To create a more walkable environment, it is key to minimize building setbacks - the distance between the edge of the public right-of-way and the front of a building. Buildings that are adjacent to a sidewalk provide pedestrians direct connections to destinations and create an enlivened urban space with opportunities to activate ground-floor uses and provide shopping or entertainment offerings.

Smaller setbacks are characteristic of Mixed-Use Local, or Urban/Suburban Local street typologies, which prioritize pedestrian-oriented design. Meanwhile, larger setbacks that can accommodate front-end parking are more characteristic of Secondary Arterials (whether suburban, industrial, mixed-use, or urban). Primary Arterials are designed to move vehicles and may require larger setback minimum requirements to accommodate additional parking in front of buildings, and to provide a buffer from fast-moving traffic to any pedestrian activity.

Vehicle Speeds

A successful street hierarchy aligns travel speeds with street usages and roadway function. While major regional roads may require higher speeds to accommodate heavy traffic flows, most smaller roadways serve lower volumes of vehicles. On these roads, lower speeds can ensure a safer environment for both vehicles and pedestrians/cyclists.

Street typologies that focus on moving vehicles, such as Suburban Commercial, Residential, or Industrial Super and Primary Arterials, will feature higher speeds. However, typologies that focus on people and places—such as Mixed-Use, Multi-family, Urban/Suburban Local Roads and Secondary Arterials—will feature lower speed limits, to increase road safety for both people and vehicles.

Parking

Parking provides access to work, retail, or entertainment. Drivers in search of parking create serious traffic and safety challenges. Incorporating safe street design elements, such as buffering between bike and travel lanes, can create a safer on-street parking environment.

Street types that emphasize people and places, such as those in Mixed-Use or Multi-family contexts, restrict the supply of parking overall, which allows for more active uses, denser environments, and connectivity for pedestrians and cyclists. Meanwhile, intermediary street typologies that support vehicles as well as people and places, will likely offer some on-street parking, in addition to smaller on-site lots. Conversely, Suburban contexts or Super and Primary Arterials may eliminate on-street parking altogether, in favor of large surface lots and structures.

Curb Access

Space between the curb and travel lanes provides an opportunity to serve many uses such as driveway access, bus lanes, cycle tracks, on-street parking, bicycle parking, drop-off delivery zones, or shared mobility hubs. This flexible zone can be converted based on demand and play a role in street activation. The design of the curb influences how cars and people enter and exit work, residential, and retail environments. In areas where traffic and turning movements increase the risk of accidents, implementation strategies will reduce conflicts between driveway traffic and pedestrians, and improve roadway safety.

Suburban Commercial contexts and Super or Primary Arterials focus on the ease of ingress and egress for vehicles, while Mixed-Use or Local street typologies focus on moving people, and emphasize how vehicle access can coexist with safe, walkable environments.

Lane Width

Lane width determines how cities can allocate space for vehicles, transit, trucks, and bikes. Streets often contain dedicated space for bike lanes, parking, safety islands, and travel lanes. The lane width will vary based on the surrounding context and modal priority; transit, freight and emergency response vehicles require wider travel lanes.

Wider lane widths are designed to move vehicles in high-speed environments. For example, a Suburban Primary Arterial will be wide enough to efficiently move passengers through residential and commercial environments, while Mixed-Use Local or Suburban Local environments support narrow lanes to reduce crossing distances for pedestrians and to help promote slower, safer driving speeds.

Mobility Recommendations

Mobility recommendations were developed based on the Highway 151 and Loop 1604 Area's Vision and Goals and to address the challenges identified during the planning process. Specific strategies to implement these recommendations can be found in the Implementation section of the plan.

Mobility Recommendation #1: Continue Implementing the San Antonio Vision Zero Action Plan.

The City of San Antonio's [Vision Zero](#) initiative aims to achieve zero fatalities on the community's roadways and improve roadway safety for all users, whether driving, bicycling, or walking. The Vision Zero initiative evaluates and makes recommendations to improve safety in Severe Pedestrian Injury Areas (SPIAs) - locations where two or more crashes close together have resulted in severe pedestrian injuries. Potential tools for improving pedestrian safety in SPIAs include Leading Pedestrian Intervals, Medians, and Pedestrian Crossing Islands, based upon analysis of the unique factors that contribute to crashes in each location and depending upon the results of engineering assessments. Another approach to improve safety involves dedicating more space in the roadway to bicyclists and pedestrians. From new ways to protect bicycle lanes with separated barriers such as bollards, to landscaping and planters and raised medians, San Antonio has many available tools to improve pedestrian and bicycle safety. The City of San Antonio [Vision Zero Action Plan](#) lists additional tools for improving pedestrian and bicycle safety.

The Highway 151 and Loop 1604 Area Regional Center has a wealth of opportunities for improving transportation and safety, such as improving crossings and investing in complete streets. In particular, the [2018 San Antonio Severe Pedestrian Injury Areas Report](#) (pages 31-32) identifies a Severe Pedestrian Injury Area (SPIA) within the regional center, on Potranco Road from Loop 1604 to Rousseau Street, that

should be prioritized for study and investment.

Additional analysis of pedestrian, bicycle, and vehicle crash data, along with community input, also identified additional points of conflict between people and vehicles that should be studied for future improvements. Major highways, such as Loop 1604 and Highway 151, and their associated frontage roads, create barriers for many pedestrians and bicyclists. The unwelcoming environment of vehicular speed and limited amenities hinders travel options for those walking or bicycling. Major roadways, like Culebra Road and Potranco Road, have insufficient accommodations and less than ideal conditions for pedestrians and bicycles to either travel on or safely cross. Employing strategies to improve the experience for pedestrians can greatly increase safety in the regional center.

Mobility Recommendation #2: Complete the multimodal mobility network and establish new connections through partnerships.

Multimodal and connected networks are key aspects to providing mobility for all users, regardless of ability or financial status. Transit improvements ensure areas are accessible, as bicycle and pedestrian infrastructure provide last-mile connections to and from transit and key destinations. Urban design improvements, such as driveway relocation, street calming, and complete streets, further support transit improvements while providing safe and inviting spaces.

Balanced/Multimodal Streets

Balanced and multimodal streets, or “complete streets” are envisioned for the Highway 151 and Loop 1604 Area Regional Center, providing safe roadway designs for vehicles, pedestrians, and cyclists. The following streets are recommended for future study by the Transportation Department and Public Works Department for complete streets improvements:

- Rogers Road from Culebra Road to West Military Drive;
- Wiseman Boulevard from west of Loop 1604 to Westover Hills Boulevard; and
- North Ellison Drive from Loop 1604 to West Military Drive.

Priority Multi-Use Trails

Key Leon Creek Greenway trail connections on the periphery of the Highway 151 and Loop 1604 Area Regional Center provide critical continuous arteries of a separated pedestrian and bicycling infrastructure network, connecting numerous parks, retail locations, schools, and libraries. The critical access points to these regional amenities provide opportunities to improve connections for people who walk or bicycle, through deliberate side paths or on-road infrastructure like crosswalks, sidewalks, and bicycle facilities. New connections are envisioned along:

- Wiseman Boulevard from Loop 1604 to Westover Hills Boulevard;
- Westover Hills Boulevard from North Ellison Drive to Culebra Road;
- Easterling from Culebra Road south to Culebra Creek Greenway connecting across Culebra Creek between Easterling and Impala Summit; and
- Copperfield Road from its northern terminus at Highland Mist Lane to either Wiseman Boulevard or North Ellison Drive.
- Military Drive from Loop 1604 to Hunt Lane

Preferred Bicycle Routes

In many parts of the regional center, bicyclists currently ride in mixed traffic on high-speed and high-volume roads. Additional bike infrastructure, such as designated lanes, is highly recommended where feasible and as part of “complete streets” and other improvement projects. These measures will improve riding conditions for today’s commuters and welcome newer, less confident bicyclists that are

unaccustomed to riding alongside vehicles. The provision of bicycle parking and micro-mobility options at transit stops provides an important connection for people who want to access transit by bicycle and helps to address the “last mile” challenge. Overall, supporting bicycle infrastructure helps connect people to and from their destinations and transit.

Based on input from the Planning Team and other community stakeholders, the Highway 151 and Loop 1604 Area Regional Center Plan identifies priority bicycle routes for the regional center including:

- Copperfield Road from Rousseau Street north toward North Ellison Drive and Wiseman Boulevard;
- Rousseau Street from Copperfield Road to Potranco Road; and
- Impala Summit, connecting north to the proposed multi-use trail.

Specific corridor areas identified for balanced and multimodal complete streets, along the Rogers Road and North Ellison Drive roadways, would also incorporate bicycling infrastructure as a component of the design - based on future studies by the Transportation Department and Public Works Department - to determine feasibility and the types of facilities needed.

Mobility Recommendation #3: Manage transportation demand through targeted employer-based solutions.

While congestion can be viewed as a sign of economic health, congestion delays waste valuable time and create transportation emissions that reduce air quality. The transportation industry has learned that a community cannot build itself out of congestion by continuously adding additional lanes; however, a series of targeted strategies can provide more travel options and reduce the demand on its roadways. The shift of users from driving alone to alternative modes of transportation can alleviate congestion along a corridor or within the area. This becomes more viable when alternatives are convenient and integrated into the daily life of area residents and employees. To support economic and residential growth and continue the vibrancy of the region’s economic centers, the community needs easy and reliable choices for traveling to and from work, school, and other key destinations.

Employer-based, public-private collaborations, such as an on-demand, shared-ride shuttles between major employment campuses, retail locations, and transit hubs ensure connectivity throughout the regional center. Daytime shuttles provide options for employees who leave their job site for lunch or run errands. Other employer-based policies, such as telecommuting, compressed workweeks, and flexible schedules can reduce the demand on the transportation network.

Mobility Recommendation #4: Support VIA Metropolitan Transit service by prioritizing transit supportive policies and infrastructure near transit facilities, stations, and stops.

Efficient and reliable VIA Metropolitan Transit service depends on transit-supportive policies and infrastructure, such as a cohesive network of accessible sidewalks, crosswalks, and curb ramps improvements, to provide safe connections to transit facilities for pedestrians, cyclists, or those getting dropped off from a vehicle.

Providing last-mile connections between transit and key destinations, such as jobs and public spaces, improves mobility throughout the regional center while supporting walkability and safety for all transportation users. Key components of VIA’s approach of making a place transit-supportive are streets designed for pedestrians, improving the safety of all users, and supporting compact, mixed-use developments that provide access to a variety of services reachable on foot.

Every person that gets on or off a bus or other transit vehicle is a pedestrian. Safe, comfortable, and direct access to transit for people who walk or bike to a transit station or stop will improve their experience as a transit rider and will increase the number of people who choose to walk, cycle, or take transit as their preferred travel choice. These improvements also contribute to the overall quality of neighborhoods and communities.

Key improvements that could improve transit mobility include bus-only lanes that give priority to buses in times of peak hour traffic; queue jump traffic signals that allow buses to advance ahead of traffic; transit priority lanes; and bus bulbs to allow buses to pick up passengers without entering/exiting a travel lane. Studies will need to be conducted to determine the appropriateness for each strategy for the areas of local congestion, including:

- Loop 1604 from Military Drive to Wiseman Road;
- Wiseman Road from Loop 1604 to North Ellison Drive; and
- North Ellison Drive from Northwest Vista College to Westover Hills Boulevard.

Amenities and Public Space

[See Figure 2122: Amenities and Public Space Framework Map]

Introduction

Incorporating amenities and public space throughout the Highway 151 and Loop 1604 Area Regional Center is a key factor in creating a destination and sense of community. New commercial business and campus style developments will appreciate amenities such as nearby restaurants and plazas for their employees to take lunch breaks or enjoy the outdoors. Residents in both existing and new residential developments will also look for open spaces for their children to play, high quality sidewalks to walk with their dogs, and other public spaces like community centers to gather with their neighbors.

Amenities and public space improvements can establish a character for the regional center through elements such as public art, signage or wayfinding to direct visitors to notable destinations, and plazas for people to gather with their neighbors, fellow students, or coworkers. Features such as streetscape improvements, enhanced lighting, priority connections, and trails can increase safety for residents, employees, students, and visitors who might walk or bike in the area. Collectively, amenities and public space improvements are the missing link that make the regional center a unique place and destination.

While highway access to the regional center has contributed to it being one of the fastest growing areas in the city, rapid development and the dependency on highways and a few smaller streets to move about has led to a disconnect in overall character and accessibility. The mobility recommendations within this plan attempt to knit together the area overall, but additional considerations around amenities and public space can further connect the area and make it easier for those who live, work, or play here to get around safely and connect with their community. To further create a sense of identity, incorporating public art into the planned trail system and along the Highway 151 and Loop 1604 Area roadways can establish more sense of community among residents, employees, students, and visitors.

Amenities and Public Space Components

Parks, Trails, and Open Space

The Highway 151 and Loop 1604 Area Regional Center has a limited amount of publicly accessible green space within its boundary. This deficiency was one of the most commonly mentioned comments among community members and the Planning Team throughout this planning process. The SA Parks System Plan has a goal of providing parks and trails within a 10-minute walk for residents. Since this regional center is not adequately served by parks, more coordinated efforts are needed to communicate to landowners the need for parks space, how that could align with their development goals, and incentives available for park or open space dedication.

Programs should be developed to increase awareness of Parks & Recreation Department resources and programs related to park dedication and creation and to increase coordination between the Parks & Recreation Department, property owners, and developers. As more developments occur within the [regional center](#), priority should be given to those that increase the amount of publicly accessible space within the [plan](#) area. This does not necessarily need to be park space, but could be a public plaza or gathering area with shops or restaurants. Alternatively, the development could include community-accessible meeting rooms or other public community space.

The lack of publicly-owned land in the area makes it difficult to designate specific park locations at this time. However, there are a number of floodplain and drainage areas that provide ample opportunities to create a linear park and trail network between campus developments and existing urban trails, and that could expand beyond the regional center on Culebra Creek and connect to the overall Howard W. Peak Greenway Trail System

Streetscape Improvements

The regional center includes many major corridors that must accommodate motor vehicles, pedestrian, bicycle, and public transit usage. The mobility section of this plan identifies street types based on the intersection of roadway function and adjacent land use and recommends further studies and investments according to the priorities identified for various area streets. Additionally, in order to make pedestrian and bicyclist activity safer throughout the area, it is crucial to enhance many streetscapes by widening sidewalks, adding more defined crosswalks, increasing/adding street lighting, and incorporating more landscaping and street trees. The community wants to be able to safely access nearby uses, including greenways and residences, and these features are crucial to creating this type of environment.

Character Defining Features

The Highway 151 and Loop 1604 Area Regional Center has several existing character defining features that contribute to a “natural” feel in the area. Specifically, the Highway 151 Gateway Corridor zoning overlay district requires certain aesthetic regulations, such as a minimum building setback, limitations on sign height and size, and requirements for greenery or screening of parking lots. These design considerations allow for the wealth of economic activity that takes place in the area to occur largely behind trees and other natural landscaping. Maintaining this natural feel is a strong desire of the Planning Team and community members, but they would also like to see more park space and other amenities created in the area. Highlighting the prominent corridors, regional access, and creating opportunities for internal trails will provide much needed space for recreation that is missing in the regional center. The regional center’s role as a major economic hub and tourist attraction creates valuable opportunities for public art or signage that create a cohesive identity for the area. This process should involve major anchor institutions and stakeholders as well as residents and visitors to the area to encourage a cohesive feel.

Amenities and Public Space Recommendations

Amenities and Public Space recommendations were developed based on the Highway 151 and Loop 1604 Area’s Vision and Goals and to address the challenges identified during the planning process. Specific strategies to implement these recommendations can be found in the Implementation section of the plan.

Amenities and Public Space Recommendation #1: Create more public green space wherever feasible, including the continuation of the Howard W. Peak Greenway Trail System through Culebra Creek.

Throughout meetings with the Planning Team and community members, the lack of area park space was one of the most consistent topics brought up by residents. Given the lack of accessible park and open space in the regional center, this plan recommends that future areas be identified, perhaps those within utility easements or floodplain areas, as potential greenways and publicly accessible open spaces. The Howard W. Peak Greenway Trail System has a planned expansion from Cathedral Rock Park to Culebra Creek Park to complete the Leon Creek Trail system, which should be encouraged for the area.

[See [Figure 22-23](#): Conceptual Rendering of Leon Creek Greenway Expansion]

Amenities and Public Space Recommendation #2: Establish character defining elements unique to the regional center.

Proposed amenities and public space improvements are key for creating a defining character for the Highway 151 and Loop 1604 Area Regional Center. Public art, such as statues, monuments, sidewalk art, murals, and crosswalk design, along the Highway 151 and Loop 1604 roadways and the proposed internal trail network will help establish these areas as recognizable corridors in the regional center. Incorporating public art and wayfinding of varying scales that has a cohesive theme will help unify neighborhood, commercial, and natural areas that are currently disconnected in character and design.

Public art is an element that creates cohesion throughout the regional center, but it also provides a sense of belonging for residents and boosts community pride. Well-designed public art and wayfinding can also serve as an economic development driver. It creates destinations within the regional center, potentially bringing in visitors from throughout the city. The theme of these character defining elements could highlight the origins or existing attributes of the regional center, such as the natural landscape and tree canopy in the area.

Amenities and Public Space Recommendation #3: Create more areas and opportunities for community gathering.

Residents and workers in the area stressed a desire to have more places to gather, such as a public community center or perhaps a private mixed-use development with shops, restaurants, and a plaza space for gathering. As the area grows, there will be an increased need for more gathering places and community amenities. While many of the single-family neighborhoods already have areas such as parks or clubhouses, these kinds of spaces are not available to others within the area. Several areas are identified for Regional Mixed-Use within the future land use map - these areas would be ideal locations for gathering spaces or other community amenities.

Housing

[See Figure 2324: Housing Snapshot]

[See Figure 2425: Percent of Cost Burdened Households by Income and Tenure, 2018]

Housing Snapshot

The Highway 151 and Loop 1604 Area Regional Center has a population of 13,542 within 4,871 households. The plan area has experienced significant housing growth over the past two decades and has outpaced the city's rate of growth, particularly between 2010 and 2018 when the area's 4.9% annual growth rate was over four times the citywide average. While this represents strong growth of approximately 200 housing units per year since 2010, this is slower growth than the forecasted average annual increase to 2040. Between 2010 and 2040, the plan area is forecast to grow by 11,300 households.

Compared to the rest of San Antonio, households in the regional center are more likely to be family households with children. The majority of households (74%) are family households, which is higher than the citywide share. As a result, the average household size is greater in the regional center. The higher share of family households also results in a lower median age for the population in the plan area compared to the metropolitan region. The regional center's median age is 32.7 years, compared to 34 years for San Antonio as a whole. Not only is there a higher share of children in the plan area, but there is also a lower share of senior residents (those over the age of 65), which also contributes to the lower median age.

The regional center population has a higher rate of educational attainment than the citywide average and a higher average household income than the region as a whole. Of the plan area population aged 25 and older, only 6% have less than a high school diploma (compared to 18% in the city), and 52% have an Associate, Bachelor's, or Graduate/Professional degree (compared to 34% in the city). The average household income in the regional center is \$99,500, compared to \$70,000 for the City of San Antonio.

The housing stock in the Highway 151 and Loop 1604 Area Regional Center is predominately single-family detached homes (72%). Prior to 2010, housing development in the regional center was mostly single-family homes. However, since 2010 the majority of new housing development has shifted toward multi-family housing. This is likely because the plan area has built-out and land for less dense housing products is limited.

In terms of housing tenure, the Highway 151 and Loop 1604 Area Regional Center has a similar proportion of owner-occupied housing units to that of the city overall. Slightly more than half (54%) of the homes in the regional center are owner-occupied, which is comparable to San Antonio's owner-occupied rate of 53%. However, rental units have increased in the plan area in recent years.

Compared to Bexar County, housing costs for the regional center are higher. The average home list price for the plan area is \$213,500 (as estimated by Zillow for the 78251 zip code area). However, newer homes built in the plan area since 2010 are selling for much higher amounts, ranging between \$200,000 and \$600,000. The average rental rate for an apartment in the plan area is \$1,115 per month or \$1.19 per square foot. This is higher than the countywide average of \$952 per month and \$1.11 per square foot. Since 2017, three projects in the area have been completed totaling over 700 multi-family units.

Despite higher than average housing costs in the regional center, there is a lower share of households that are cost-burdened than the Bexar County average. Cost burden is a measure of housing costs compared to income. Households that spend more than 30% of their income on housing are considered cost-burdened. Over one-third of renter occupied households in the plan area (36%) are cost-burdened. This is lower than the Bexar County average (46%) and has been consistently lower since 2000. The rate of cost-burdened owner households (18%) is also less than the county average and has decreased since 2000. However, the share of cost-burdened households is not equal across incomes, with the vast majority of renters and owners in the regional center who earn less than \$35,000 paying more than a third of their income toward housing. This measure indicates that residents of the regional center can, on average, afford area housing prices/rates. However, the rental rates and housing prices are significantly higher than rates that are affordable for much of the plan area's workforce, especially workers in the retail or tourism industries.

Housing Challenges in the Highway 151 and Loop 1604 Area Regional Center

During the plan process, the Highway 151 and Loop 1604 Area Planning Team members were asked to assess the regional center's housing challenges during discussions at the two Planning Team meetings devoted to housing and economic development. Planning Team members were presented with relevant existing conditions data, and asked to qualitatively describe the plan area's strengths and challenges. These conversations helped identify specific housing challenges that need to be addressed in the plan as well as missing housing types the plan can help encourage.

[See [Figure 2526: Planning Team Housing Challenges Analysis](#)]

There are three main housing challenges that were identified for the Highway 151 and Loop 1604 Area Regional Center.

- **Managing Impacts of Growth** – As the regional center continues to build out and as more people are living, working, and visiting the plan area, investments and efforts are needed to address the impacts of increased activity. Efforts such as congestion mitigation, alternative mobility options, access to parks and recreation, and others are needed to ensure that quality of life in the plan area is maintained.
- **Housing Diversity** – The regional center is a desirable place to live due to its housing stock, proximity to employment, quality schools, and overall natural aesthetic. However, the housing stock is predominately split between two options: larger single-family detached homes or luxury multi-family apartments. A greater diversity of housing is needed to support retention of empty nesters and seniors, to attract younger workforce to the plan area, and to provide options for current workers in the plan area to live closer to their place of work.
- **Community Gathering Destinations** – The Highway 151 and Loop 1604 Area Regional Center lacks community amenities and gathering places that are open and accessible to all residents, workers, and visitors. Several of the plan area neighborhoods have private community amenities that do support community events and gatherings. However, these spaces are largely only open to residents of specific neighborhoods, and not publicly accessible. Therefore, there is a need for more public open spaces and gathering destinations.

Housing Recommendations

Housing recommendations were developed based on the Highway 151 and Loop 1604 Area's vision and goals, existing conditions data, and to address the challenges identified during the planning process. Specific strategies to implement these recommendations can be found in the Implementation section of the plan.

Housing Recommendation #1: Encourage higher density multi-family uses in mixed-use centers near retail, services, and access to transportation systems.

The expected employment growth in the regional center and the attractiveness of the area will continue to drive demand for housing. To promote mixed-use centers, new multi-family housing should be better integrated within other uses near major transportation routes and transit services. This will support more walkable, mixed-use places and limit adverse impacts on the transportation network in the plan area.

Housing Recommendation #2: Attract a greater diversity of housing products and affordable price points that can evolve with changing demographic conditions.

As the population in the plan area increases and as existing residents age, there will be demand for housing product types that are not oriented to large families or younger renters. These types could include patio homes, townhomes, condos, and senior housing options. Additionally, it is difficult for some of the plan area workforce to afford housing near their place of work. More affordable housing options are necessary to allow more people to live and work in the regional center, which can mitigate traffic congestion.

Economic Development

Introduction

Located on the western edge of San Antonio, the Highway 151 and Loop 1604 Area Regional Center has emerged as a major tourism destination as well as a hub for large corporate office campuses. Commonly referred to as Westover Hills, much of the plan area was master planned in the 1980s and 1990s. The regional center has several major economic anchor businesses, attractions, and institutions including SeaWorld San Antonio, Northwest Vista College, Capital Group, the Wells Fargo Wiseman Campus, Frost Bank, and CHRISTUS Santa Rosa Hospital – Westover Hills.

In 2018, the Highway 151 and Loop 1604 Area Regional Center had approximately 11,900 jobs. The plan area has a diversity of employment with a large number of jobs in the tourism, finance, health care and education industries. One out of four jobs in the plan area are within the arts, entertainment, and recreation sector. This is likely the largest employment sector due to the presence of major destinations such as SeaWorld San Antonio and the Hyatt Regency Hill Country Resort and Spa. The Finance and Insurance industry has grown substantially in recent years due the presence of the large Wells Fargo and Capital Group campuses, and the attraction of other corporate campus developments to the plan area. The CHRISTUS Santa Rosa Hospital – Westover Hills and Northwest Vista College contribute to sector diversity within the employment base, providing a large number of health care and education jobs, respectively.

Medium-wage jobs make up most of the employment in the Highway 151 and Loop 1604 Area Regional Center. Higher wages are concentrated in the finance and insurance sectors, while there are lower wage jobs with earnings of around \$15,000 per year within the retail and tourism-oriented sectors. Most people employed in the plan area do not live within the plan area. However, most workers come from relatively close by, with 56% of workers in the plan area commuting less than 10 miles.

The regional center is a major destination for office and retail developments. The plan area continues to be a sought-after location for office users, especially businesses seeking secure campuses with reliable high-speed internet or higher electrical load capacity. Since 2005, office space inventory in the plan area has increased by nearly 7% annually, and today the plan area has roughly 3.9 million square feet of office space. Retail space inventory has grown at a higher rate (9.9% annually since 2005) than office space. The plan area's 1.9 million square feet of retail space is primarily oriented to Loop 1604, and not along Highway 151 where the majority of office space is located. Just west of the plan area, in unincorporated Bexar County, is a large power center, or major retail hub, called The Alamo Ranch Shopping Center with 464,000 square feet of retail.

Hospitality is also a considerable economic driver for the area, due to proximity to SeaWorld San Antonio, easy access to Six Flags Fiesta Texas, and the "Hill Country feel" in the area. There are six hotel properties spread throughout the Highway 151 and Loop 1604 Area Regional Center including the large Hyatt Regency Hill Country Resort and Spa and the Hilton San Antonio Hill Country.

From 2010 to 2040, the regional center is forecast to grow by 24,600 jobs. Recent trends in office and retail development, as well as the growth of other anchor institutions such as CHRISTUS Santa Rosa Hospital – Westover Hills, indicate that employment growth in the plan area is on pace to meet or slightly exceed the long-term forecast.

Economic Challenges to Address

During the planning process, the Highway 151 and Loop 1604 Area Planning Team members were asked to assess the regional center's economic challenges during discussions at the two Planning Team meetings devoted to economic development and housing. Planning Team members were presented with relevant existing conditions data, and asked to qualitatively describe the plan area's strengths and challenges. These conversations helped identify specific economic challenges that need to be addressed in the plan as well as the economic opportunities the plan can help encourage.

[See **Figure 2627: Planning Team SWOT Analysis**]

The Highway 151 and Loop 1604 Area Regional Center has many economic assets and strengths, particularly infrastructure advantages. The plan area has access to desirable electrical connections and the presence of high-speed fiber-optic lines. The regional center is surrounded by a growing, desirable area for housing within the region, and employers in the plan area have been able to attract a diverse and younger-than-average workforce.

The Planning Team identified three main economic challenges for the Highway 151 and Loop 1604 Area Regional Center:

- **Access and Congestion** – The plan area is well-connected by automobile travel to the rest of San Antonio, with access from Loop 1604 and Highway 151. However, these two roads are the primary access roads and therefore attract a significant amount of internal car trips. These two major highways are congested during peak hours and need continued improvements, as well as transportation alternatives, to support the connectivity both within this regional center and also to other housing and employment centers throughout the city. Within the plan area, movement and access is constrained by a lack of connecting roadways, as well as the presence of large superblocks. Improved mobility routes for cars and all transportation modes are needed to increase access for residents, workers, and visitors. As employment increases in the plan area, access and congestion will need to be addressed in multiple ways.
- **Room to Grow** – The regional center has existing vacant sites to capture new employment. However, some sites are impacted by drainage concerns and flood ways. Creative solutions are needed to maximize capacity for employment uses in the regional center while maintaining the natural systems that exist. These natural constraints could also be considered opportunities for open space.
- **Diversity of Use and Urban Form** – Developments within the regional center are primarily large superblocks and secure campuses. This type of urban form creates a greater reliance on major arterial roadways and highways for access. Because of this type of development, there are also long distances between uses and destinations, which leads to a lack of desirability for using other modes of transportation (walking, biking, etc.). The retail and entertainment destinations are largely auto-oriented and the regional center lacks destinations where multiple uses or businesses can be accessed by foot. The lack of a mixture of uses and destinations may make the continued attraction of businesses and workforce more difficult as other employment locations

in the region move towards placing employers closer to and mixed within areas with retail, restaurants, entertainment, and housing options.

Target/Opportunity Industries

Target industries and economic opportunities were identified for the Highway 151 and Loop 1604 Area Regional Center. The target industries and economic opportunities are meant to help organize the city's economic geography and provide guidance on the role the Highway 151 and Loop 1604 Area can play in the City's overall economic development efforts. These identified industries can also give direction to the City and its economic partners as to what areas are best suited for certain opportunities when they arise. The target industries and economic opportunities for the Highway 151 and Loop 1604 Area Regional Center are:

- **Technology Campus Mixed-Use Developments** – The Westover Hills area has attracted a number of financial firms and other industries to larger, secure campus to support their technology and back office support service needs. The Hill Country and secure campus aesthetic is a major attractor for additional businesses. This cohesive aesthetic should continue to be encouraged. However, there are ways to better integrate these secure buildings and campuses into a more mixed-use environment. As employers are increasingly looking for areas that have services and amenities attractive to their workforce, encouraging mixed-use will help increase the appeal of the plan area. The Planning Team believes this could include manufacturing types of uses, if they are “clean” or technology focused, and not likely to pollute the air or water. These types of uses would also be taking advantage of the high-speed fiber internet and strong electrical grid within the area.
- **Education and Career Growth** – The Northwest Vista College is a major asset for the regional center and the city. There is the opportunity to leverage the programs offered by the College to help support the career growth of workers in the regional center.
- **Live, Work, Play Center** – The Highway 151 and Loop 1604 Area lacks destinations for retail, entertainment, and food to support workers and residents. The employment and residential uses in the plan area will support additional mixed-use destinations and gathering places with an entertainment, shopping, or recreation focus.

Attributes of a Vibrant Economic Place

Successful economic places have common attributes that help create their success. These attributes help to build upon the traditional attributes of attractive employment locations to create a cohesive strategy for managing these places. Each regional center is being evaluated based on the presence of the following attributes.

[See **Figure 2728: Attributes of a Vibrant Economic Place**]

The Highway 151 and Loop 1604 Area has many of the attributes that help create successful, vibrant economic places. The findings of the assessment for these attributes are detailed below, and identified shortcomings are addressed in the recommendations at the end of this section.

- **Brand/Identity** – The regional center has an identity and character aesthetic that has been created due to master planned development and a gateway corridor zoning overlay district. The identity of the plan area is one of large and secure campuses and a “Hill Country” natural

aesthetic. Maintaining this character will preserve the appeal of the plan area for businesses. Embracing a name for the plan area, such as Westover Hills, and building on this identity will help increase awareness.

- **Organizations** – Successful office employment areas often benefit from a collective effort between businesses, property owners, workers, and municipalities to manage and promote these areas. There are a variety of organizational structures that could be implemented. Typically, these entities focus on economic development, transportation management, area promotion/branding, and/or public space/amenity management to support the economic growth of the area while addressing major issues. The Highway 151 and Loop 1604 Area would benefit from the creation of an entity that would manage promotion of the area and advocate for transportation solutions.
- **Anchor Institutions** – The regional center has a wealth of anchor institutions in a diversity of industries, including medical (CHRISTUS Santa Rosa Hospital – Westover Hills, upcoming Methodist Hospital), finance (Capital Group, Wells Fargo, One Frost) and entertainment/hospitality (SeaWorld San Antonio, Hyatt Regency Hill Country Resort and Spa).
- **Urban/Design Planning** – Since development of the area was largely through master plans, the plan area has a cohesive feel that has helped enhance the “Hill Country” aesthetic. However, the regional center lacks distinct destinations and gathering places/community centers that bring workers, residents, and visitors together.
- **Transportation** – The transportation network in the area is highly dependent on Highway 151, Loop 1604, and major arterial roads. The large super blocks make internal mobility in the plan area difficult and should be addressed. In addition, the capacity and congestion issues on the two major highways directly impact the ability of the plan area to expand jobs and housing.
- **Investments** – The regional center has been built primarily through private investment. Improvements to the major highways have helped support growth but there is a need for additional investment. The private sector is not likely to fully fund the public infrastructure and amenities that are missing in the area.
- **Finance/Incentives** – No public financing tools have been leveraged in the plan area. The strength of the market in the plan area does not indicate the need for incentives aside from perhaps the creation of affordable housing options. Investment districts, such as a Public Improvement District (PID), could fund improvements desired by the residents and businesses in the plan area that are not likely to be funded through the City’s capital improvement program.

Economic Development Recommendations

Economic Development recommendations were developed based on the Highway 151 and Loop 1604 Area’s Vision and Goals and to address the challenges identified during the planning process. Specific strategies to implement these recommendations can be found in the Implementation section of the plan.

Economic Development Recommendation #1: Support the growth of the plan area’s and Greater: SATX’s target industries including cyber security, technology, financial services, and tourism.

The Highway 151 and Loop 1604 Area Regional Center has a diverse mixture of employers and is a premier location for IT/Technology, financial services, and back office support services in San Antonio.

Efforts to build on these strengths are needed. The plan area is also a major tourism destination, which presents an opportunity for the attraction of additional visitors and businesses.

Economic Development Recommendation #2: Attract mixed-use destinations with a focus on entertainment, retail, and services oriented to area workers and residents.

Despite the concentration of large employers and surrounding neighborhoods, the regional center lacks destinations for entertainment and retail/services oriented to the workers and residents in the plan area. Efforts are needed to attract mixed-use entertainment and retail developments to the Regional Mixed-Use areas designated in the land use plan.

Economic Development Recommendation #3: Maintain Hill Country aesthetic and office campus opportunities for employers within a more integrated land use pattern that better connects campuses to mixed-use destinations and mobility options.

The land use pattern in the regional center is dominated by large users on secure campuses and super blocks that make mobility within the regional center difficult and dependent on the major highways and arterials in the plan area. Efforts to integrate more internal connections and a smaller block pattern will support more mobility options and help to address congestion issues that may limit future economic growth. A more integrated land use pattern will also allow employers to locate closer to retail and services that are desired by and attractive to their workforce. Integration of other uses within large campus blocks can help the plan area maintain its competitive footing for attracting new businesses.

Economic Development Recommendation #4: Provide opportunities for smaller scale, multi-tenant office and employment spaces and buildings to allow for a wider-variety of types and sizes of businesses to locate in the plan area.

The regional center has many large businesses on large campuses. However, the plan area lacks smaller, multi-tenant office and employment spaces. Efforts are needed to attract multi-tenant offices to diversify the types of businesses in the plan area while supporting smaller businesses to locate near the large anchor businesses in the center.

5 Neighborhood Profiles and Priorities

What is the neighborhood profile and priorities?

San Antonio is a city of neighborhoods, each with its own unique history, qualities, and character. Some neighborhoods throughout the city have developed Neighborhood or Community Plans that reflect local values and priorities. These plans, adopted by the City, have guided local investments and improvements for many years and helped strengthen the relationship between residents and the City.

The City is currently in the process of creating Sub-Area Plans to implement the SA Tomorrow Comprehensive Plan. These Sub-Area Plans are intended to provide a more coordinated, efficient, and effective structure for neighborhood planning. The Sub-Area Plans are intended to increase equity citywide, by ensuring that all of San Antonio's neighborhoods have a base level of policy guidance, as many neighborhoods within the city lack an existing Neighborhood/Community Plan or a registered neighborhood organization. In this way, each Sub-Area Plan will integrate key elements of existing Neighborhood/Community Plans for those neighborhoods that have a plan, while promoting citywide policy consistency and providing key recommendations and strategies for those neighborhoods currently lacking that direction.

Typically, the Neighborhood Profile and Priorities section of the Sub-Area Plans provides special attention to prior neighborhood planning efforts and recognizes the community groups and individuals who were instrumental in their creation. However, the Highway 151 and Loop 1604 Area does not encompass any areas that have previously adopted Neighborhood or Community Plans. Therefore, we adjusted our approach for sub-areas that do not have previous plans.

How was it developed?

Based on conversations with Planning Team members and iterations of feedback, the Project Manager for the Highway 151 and Loop 1604 Area Regional Center worked to establish the following snapshot and priorities, generally, for the area's neighborhoods. Much of the same vision and goals that the neighborhoods have for their community is captured in the vision and goals for the plan overall.

Registered Neighborhood Associations within the Highway 151 and Loop 1604 Area Regional Center:

- North San Antonio Hills Homeowners Association
- Stonegate Hill at Westover Hills HOA
- Mountain View Acres Neighborhood Coalition
- Charro Legacy Trails Cell 4 HOA (Mirabella at Legacy Trails)
- Westover Valley HOA
- Oak Creek Community Association
- Reserve at Culebra Creek HOA

Neighborhood Snapshot

Within this regional center there are seven registered neighborhood associations. The earliest neighborhood developed was the Mountain View Acres neighborhood (at Culebra Road and Rogers Road), which still had some unpaved roads and a handful of houses in the 1960s. North San Antonio Hills (just west of Loop 1604) and Oak Creek (west of SeaWorld San Antonio and south of Military Drive) were developed soon after in the 1970s and 1980s. These three neighborhoods have the "older" suburban development style with larger lot sizes, no sidewalks, and customized construction.

Other major residential developments did not start happening until the 1990s, after the opening of SeaWorld San Antonio and the Hyatt Regency Hill Country Resort and Spa. Stonegate Hills at Westover Hill, Westover Valley, and Mirabella at Legacy Trails were all developed in the 1990s and mid-2000s, with smaller lot sizes and slightly more homes per acre. The Estates of Westover Hills, Creekside, and other Legacy Trails developments were also completed in the same time period, and the Aviara and Aviara Enclave subdivisions were built in the 2010s, but these neighborhoods do not have registered associations. In recent years, numerous apartment developments have been built around the area, in addition to several senior assisted living centers.

Strengths:

1. Maintenance of “natural feel” within developments, and Hill Country character
2. Ample restaurant and retail options close by
3. Access to Loop 1604 and Highway 151
4. High-performing schools

Opportunities and Challenges:

1. Increasing traffic congestion
2. Residents fear loss of “natural” or “country” feel to area with increased growth
3. Some older neighborhoods are not connected to sewer and remain on septic systems
4. Lack of affordable housing within the regional center, and much more housing growth outside of regional center into Alamo Ranch.
5. Lack of greenspace and parks

Priorities:

- **Neighborhood Character:** Preserve the character of neighborhoods through the use of buffers, setbacks, and appropriate transitions between different zoning districts.
- **Environmental Resiliency:** Protect the environment by planning for water usage and the impact of stormwater runoff, maintaining and improving the tree canopy, and encouraging private use of renewable energy sources such as residential solar panels.
- **Housing Choice and Affordability:** Provide housing opportunities and affordability through the development of a variety of housing types and increased acceptance of housing assistance programs.
- **Connected and Safe Transportation System:** Improve transportation options for vehicles, cyclists, and pedestrians by facilitating long-term planning and development of transportation systems, including connections to railways and connectivity across highways.

6 Implementation

Plan Purpose

This Plan proposes a medium-term vision, recommendations, and strategies for improving and developing the Highway 151 and Loop 1604 Area Regional Center Plan over the next ten years. The Plan is an implementation component of the City of San Antonio's SA Tomorrow Comprehensive Plan. Adopted in 2016, the Comprehensive Plan is the City's long-range land use and policy plan that is intended to be a blueprint for future growth and development through the year 2040. In addition to implementing the Comprehensive Plan, the Highway 151 and Loop 1604 Area Regional Center Plan aims to improve quality of life for San Antonio residents and guide growth and development to accommodate projected housing and employment increases. The Highway 151 and Loop 1604 Area Regional Center Plan provides an equitable path for all neighborhoods to participate in planning, to create priorities, and to advocate for implementing their priorities in the future.

Intent of the Plan

The Highway 151 and Loop 1604 Area Regional Center Plan will be the essential tool to guide future development and City investment in the plan area. The community-based planning process resulted in achievable recommendations and strategies that will be used by City departments, partner agencies, private entities, and community partners to support livable, complete neighborhoods.

How to Use This Plan

The vision for the Highway 151 and Loop 1604 Area Regional Center can be realized through implementation of the Plan Framework and Implementation Priorities, with recommendations and strategies related to the following topics: Land Use, Focus Areas, Mobility, Amenities and Public Space, Housing, and Economic Development. These recommendations and strategies include policy and regulatory matters, partnerships, and investments. Plan recommendations are written to provide actionable specificity while still allowing the flexibility needed to adapt to unforeseen challenges or opportunities.

Coordination with Adopted Plans

The Highway 151 and Loop 1604 Area does not encompass any areas that have a previously adopted Neighborhood or Community Plan. However, the Highway 151 and Loop 1604 Area Regional Center Plan was developed to complement and contribute to the implementation of the following regional and citywide plans:

- *SA Tomorrow Multimodal Transportation Plan*
- *SA Tomorrow Sustainability Plan*
- *VIA's Vision 2040 Plan*
- *SA Corridors Strategic Framework Plan*
- *San Antonio's Housing Policy Framework*

In implementing the Highway 151 and Loop 1604 Area Regional Center Plan, further consideration should be given to the recommendations of emerging and ongoing planning processes, including but not limited to:

- *VIA's Rapid Transit Corridors planning*
- *SA Climate Ready*

- *San Antonio's Housing Policy Framework implementation programs*
- *SA Parks System Plan*
- *San Antonio Sidewalk Master Plan*
- *Vision Zero San Antonio initiative*
- *ConnectSA*

Statutory Requirements

Once adopted by City Council, the Highway 151 and Loop 1604 Area Regional Center Plan becomes a component of the City's SA Tomorrow Comprehensive Plan. Where the previously adopted West/Southwest Sector land use plan is contained within or partially overlaps the Highway 151 and Loop 1604 Area Regional Center Plan, the Sub-Area Plan will be the plan of reference for land use designations. Similarly, where a previous plan and the Highway 151 and Loop 1604 Area Regional Center Plan have conflicting policies or priorities within the adopted boundary of the Highway 151 and Loop 1604 Area Regional Center Plan, the Sub-Area Plan will be City policy.

By virtue of the plan adoption process, all proposed projects must be found to be consistent with the SA Tomorrow Comprehensive Plan, and as such, the Highway 151 and Loop 1604 Area Regional Center Plan must be consulted when proposing a public investment or a land use project that requires deviation from current entitlements.

Implementation – Land Use

Land Use Recommendation #1: Preserve and enhance the character and identity of established neighborhoods.

Strategy 1.1 (Regulatory and Policy)

Review current zoning to ensure that the zoning within established neighborhoods is consistent with existing Residential Estate, Low Density Residential, and Urban Low Density Residential uses and future land use designations. Rezone properties that are not zoned consistently to ensure that undeveloped lots are not over-built in the future.

Strategy 1.2 (Regulatory and Policy)

Where commercial corridors abut existing residential neighborhoods, ensure that use and development of adjacent non-residential and mixed-use properties do not have a negative impact on the character of the neighborhood, or create a nuisance for its residents. Heights above two stories should be stepped back to avoid views into back yards of residential homes. Fences, buffers, and screening should be required to mitigate nonresidential noise, light, traffic, and activity. Where non-residential uses occur next to an existing residential use, front yard setbacks should be consistent with the yard space of the adjacent residences. Where commercial properties abut neighborhoods with a rural form and a dark night sky, parking lot lighting should be shielded so as to avoid light trespass into the neighborhood space.

Strategy 1.3 (Regulatory and Policy)

Regardless of future rezoning, do not waive requirements for sidewalks, pedestrian connectivity, and parkland dedication in the areas within or adjoining existing neighborhoods.

Land Use Recommendation #2: Encourage efficient density patterns with commercial and mixed-use development at appropriate scales.

Strategy 2.1 (Regulatory and Policy)

Review current zoning for consistency with the patterns in development intensity reflected in the future land use map. In particular, ensure that properties in areas designated Regional Mixed-Use or Business/Innovation Mixed-Use have been zoned to accommodate the range of uses anticipated for these respective land use categories.

Strategy 2.2 (Regulatory and Policy)

Rezone large mixed-use parcels as needed to provide necessary flexibility, while ensuring complementary uses and site design including density, building heights, set-backs, and on-site amenities. In the areas designated as Urban Mixed-Use, ensure that zoning allows for vertical mixed-use development, limited setbacks, and increased density in order to create walkable areas connected to transit and to commercial corridors.

Strategy 2.3 (Regulatory and Policy)

Promote rezoning for Transit-Oriented Development and Mixed-Use and encourage site design that includes bike parking, ride sharing, temporary parking and loading areas (for passengers and for freight), and temporary uses and popup activity such as cart vendors and food stands in areas near transit facilities.

Land Use Recommendation #3: Increase the amount of public park land, open spaces, and recreational areas wherever possible.

Strategy 3.1 (Investment)

Identify and acquire available land for public neighborhood, community, and/or regional parks.

Strategy 3.2 (Investment)

Identify and acquire available land located within stream corridor buffers, floodplain areas or land with steep slopes in order to provide public greenbelts, trails, and natural areas, particularly connecting Leon Creek Greenway through Culebra Creek.

Strategy 3.3 (Regulatory and Policy)

Explore opportunities for joint-use park and recreational facility planning with schools in the area, as this is a shared need between the City and the Northside Independent School District (NISD).

Strategy 3.4 (Regulatory and Policy, Partnerships)

Foster more effective communication between the Parks & Recreation Department and Homeowners Associations in the area to ensure that existing neighborhood parks established through restrictive covenants are being adequately maintained.

Strategy 3.5 (Regulatory and Policy)

For new nonresidential development, encourage the use of land available through setbacks, tree preservation requirements, floodplain and stormwater protection areas, and steep slopes to be designated for trails and publicly accessible open spaces.

Strategy 3.6 (Regulatory and Policy, Partnerships)

Encourage the dedication of easements, either to the City or to a private conservation organization, for future greenbelts and trails in the plan area.

Implementation – Focus Areas

Focus Area Recommendation #1: Encourage the creation of park, open, recreation, or civic space in the identified focus areas.

Strategy 1.1 (Regulatory and Policy, Partnerships, Investment)

Explore creation of development incentives to encourage parks, plazas, or other open space in new private developments in regional centers that lack adequate park space.

Strategy 1.2 (Regulatory and Policy, Partnerships, Investment)

Explore creation of park dedication requirements for large parcels in areas without adequate park space. As San Antonio continues to grow, and given that the regional centers are the areas expected to absorb much of this growth, the City should explore ways in which to ensure regional centers have adequate park space for the existing and future people who might live, work, or play in the area.

Focus Area Recommendation #2: Protect the established character of the regional center in each focus area by providing landscaped buffers and screening, and encouraging low impact development.

Strategy 2.1 (Regulatory and Policy, Partnerships)

Explore expanding the Highway 151 Gateway Corridor, or adopting additional Corridor Overlay Districts for other major corridors or the focus areas within the regional center.

Strategy 2.2 (Regulatory and Policy, Partnerships, Investment)

Coordinate with the San Antonio River Authority (SARA) to identify priority areas for green infrastructure and prioritize projects that encourage green infrastructure and other natural stormwater best management practices.

Focus Area Recommendation #3: Support the unique vision for each focus area and emerging industries within the regional center by creating high-quality places with a mix of uses, vibrant public spaces, and appropriate transitions to surrounding uses.

Strategy 3.1 (Regulatory and Policy)

Amend the UDC to create new zoning districts that will support mixed-use, pedestrian-friendly development at a variety of scales within the focus areas. In order to allow the types of mixed-use development identified for the focus areas, the UDC will need to be amended to create new zoning districts that allow for this type of development. Then, these spaces desired for mixed-use can be rezoned to accommodate the desired vision.

Strategy 3.2 (Regulatory and Policy, Partnerships, Investment)

Coordinate with VIA Metropolitan Transit to explore opportunities for transit connections within the regional center and identified focus areas.

Strategy 3.3 (Regulatory and Policy, Partnerships, Investment)

Embrace emerging industries within the regional center such as healthcare, hospitality, finance services/back office support, and retail. New focus area developments should serve existing employees, and also work to support these specializing industries coming to the regional center. This could be achieved through new mixed-use spaces for employees to go to lunch, or through additional developments that included these industries.

Implementation – Mobility

Mobility Recommendation #1: Continue implementing the San Antonio Vision Zero Action Plan.

Strategy 1.1 (Regulatory and Policy, Investment)

Continue evaluating and implementing proven strategies and best-practice improvements, to potentially include traffic calming and “complete streets” principles, which improve pedestrian, bicycle, and traffic safety and help achieve San Antonio’s Vision Zero goals. The highest priority area is the identified Severe Pedestrian Injury Area (SPIA) on Potranco Road from Loop 1604 to Rousseau Street.

Additional analysis of pedestrian, bicycle, and vehicle crash data and community input, also identified the following as priorities:

- Loop 1604 and Culebra Road; and
- Culebra Road and Easterling.

Strategy 1.2 (Regulatory and Policy, Investment)

In anticipation of adding more density to land use classifications and due to current needs, invest in well-designed crossings that incorporate best practices for safety and placemaking at the intersections of:

- Loop 1604 and Culebra Road;
- Culebra Road and Easterling;
- Rogers Road and Wiseman Boulevard;
- Rogers Road and Rogers Run; and
- West Military Drive and Escala Parkway.

Strategy 1.3 (Regulatory and Policy, Partnerships)

Partner with residents and business owners to identify opportunities to enhance the urban design of streetscapes, by consolidating driveways and/or relocating driveways to the back of buildings, to reduce the number of crashes and preserve the flow of traffic. The priority location for application of this strategy is Culebra Road from Rogers Road to Mountain View Drive.

Mobility Recommendation #2: Complete the multimodal mobility network and establish new connections through partnerships.

Strategy 2.1 (Regulatory and Policy, Partnerships, Investment)

Conduct a “complete streets” study to find opportunities to reduce pedestrian crossing time, increase pedestrian crossing opportunities, and reduce operational speeds. The following street segments are ideal for study and implementation:

- Rogers Road from Culebra Road to West Military Drive;
- Wiseman Boulevard from west of Loop 1604 to Westover Hills Blvd; and
- North Ellison Drive from Loop 1604 to West Military Drive.

Strategy 2.2 (Investment)

Implement on-road infrastructure and wayfinding projects to improve first/last mile connectivity to regionally significant trailheads such as the Terra Oak and Grissom trailheads.

Strategy 2.3 (Regulatory and Policy, Partnerships, Investment)

Continue to complete the bicycle and sidewalk network by implementing priority Multi-Use Trail projects and adding facilities on streets identified for repaving or reconstruction. As shown on the Mobility Framework Map, priority locations to implement this strategy include the intersections of:

- Loop 1604 and West Military Drive;
- Loop 1604 and Wiseman Boulevard;
- Loop 1604 and Culebra Road;
- Highway 151 and Wiseman Boulevard;
- Highway 151 and Westover Hills Boulevard; and
- Highway 151 and West Military Drive.

Strategy 2.4 (Regulatory and Policy)

Update street design standards to be consistent with SA Tomorrow goals for safety, economic growth, development and city form, and to reflect the relationship between the built environment and the streetscape. Street design standards should consider all levels of interactions with the street including pedestrians, bicyclists, and motorists.

Mobility Recommendation #3: Manage transportation demand through targeted employer-based solutions.

Strategy 3.1 (Partnerships, Investment)

Partner with employers to incentivize residents, employees, and visitors to travel by transit, bicycle, rideshare, or carshare through incentives designed to shift commutes away from single-occupant vehicle trips.

Strategy 3.2 (Regulatory and Policy, Partnerships)

Support partnerships that provide a rideshare program or on-demand circulator between major employer campuses and major transit stops.

Mobility Recommendation #4: Support VIA Metropolitan Transit service by prioritizing transit supportive policies and infrastructure near transit facilities, stations, and stops.

Strategy 4.1 (Regulatory and Policy, Partnerships, Investment)

Implement first/last mile strategies, such as sidewalks, curb ramps, crosswalks, bicycle facilities, etc., at VIA Metropolitan Transit facilities, to promote access to transit by creating inviting, quality public space around transit facilities, stations, and stops where large numbers of people can benefit. The priority location for these investments is near Northwest Vista College on Wiseman Boulevard between North Ellison Drive and Highway 151.

Strategy 4.2 (Regulatory and Policy, Partnerships, Investment)

Evaluate street and intersection design for transit reliability chokepoints, like vehicular traffic, and prioritize multimodal investments to ensure reliable alternatives to vehicular travel. Targeted interventions that could positively impact transit service reliability include:

- Loop 1604 from Military Drive to Wiseman Road;
- Wiseman Road from Loop 1604 to North Ellison Drive; and
- North Ellison Drive from Northwest Vista College to Westover Hills Boulevard.

Implementation – Amenities and Public Space

Amenities and Public Space Recommendation #1: Create more public green space wherever feasible, including the continuation of the Howard W. Peak Trail System through Culebra Creek.

Strategy 1.1 (Partnerships, Investment)

Coordinate with the Parks & Recreation Department to ensure completion of the Leon Creek Greenway system through Culebra Creek.

Strategy 1.2 (Regulatory and Policy, Partnerships, Investment)

Explore the creation of development incentives and expanded park dedication requirements to encourage projects that increase publicly accessible open space (parks, plazas, trails, etc.) within the regional center, especially in areas designated for Regional Mixed-Use or Business/Innovation Mixed-Use.

Strategy 1.3 (Regulatory and Policy, Partnerships, Investment)

Work with the Parks & Recreation Department to more fully convey opportunities for property owners and developers to dedicate land for parks, plazas, or other recreation space coordinated with the Parks & Recreation Department's mandates.

Amenities and Public Space Recommendation #2: Establish character defining elements unique to the regional center.

Strategy 2.1 (Regulatory and Policy)

Encourage the adoption of further zoning regulations throughout the plan area to encourage the type of development already occurring in the Gateway Corridor district.

Strategy 2.2 (Partnerships, Investment)

Coordinate with the Department of Arts & Culture, major anchor and tourism institutions within the regional center, and neighborhood associations to identify opportunities for public art or wayfinding signage.

Amenities and Public Space Recommendation #3: Create more areas and opportunities for community gathering.

Strategy 3.1 (Regulatory and Policy, Partnerships)

Prioritize mixed-use developments that include community gathering spaces such as public open spaces, plazas, or meeting rooms.

Implementation – Housing

Housing Recommendation #1: Encourage higher density multi-family uses in mixed-use centers near retail, services, and access to transportation systems.

Strategy 1.1 (Regulatory and Policy, Partnerships)

Work with vacant property owners and developers to integrate multi-family developments into larger mixed-use projects or areas. Require projects to orient to streets and pedestrian paths and provide connections to adjacent and nearby commercial areas.

Strategy 1.2 (Regulatory and Policy, Partnerships)

Promote higher density multi-family housing in a mixed-use environment near VIA Metropolitan Transit bus infrastructure to support transit-oriented development.

Housing Recommendation #2: Attract a greater diversity of housing products and affordable price points that can evolve with changing demographic conditions.

Strategy 2.1 (Regulatory and Policy, Partnerships)

Partner with local affordable housing providers and developers to identify potential sites and projects for development of affordable and mixed-income housing. Prioritize sites in focus areas and along mixed-use corridors. Consider proactively purchasing sites for future affordable housing development through the use of a community land trust, and/or through partnership with San Antonio Housing Authority (SAHA) and other non-profit housing partners to ensure affordable units are available in the area.

Strategy 2.2 (Regulatory and Policy, Partnerships)

Work with vacant property owners and developers to integrate a greater diversity of housing types in the regional center with a focus on attracting attached and small lot for-sale housing and senior-oriented housing options.

Implementation – Economic Development

Economic Development Recommendation #1: Support the growth of the plan area's and Greater: SATX's target industries including cyber security, technology, financial services, and tourism.

Strategy 1.1 (Partnerships)

Work with area businesses and other economic development entities to explore options and interest in the formation of a place management organization, transportation management association, and/or economic development partnership to support the future growth and economic health of the plan area with a focus on transportation issues, economic and workforce development, and placemaking.

Strategy 1.2 (Partnerships)

Support partnerships and programs between Northwest Vista College and area businesses to create and tailor opportunities for workforce education and training that support target industry businesses in the regional center.

Economic Development Recommendation #2: Attract mixed-use destinations with a focus on entertainment, retail, and services oriented to area workers and residents.

Strategy 2.1 (Regulatory and Policy)

Encourage the creation of mixed-use centers along Highway 151 with retail and entertainment uses oriented to the plan area workforce and residents. Explore potential development incentives (such as density bonus and other regulatory benefits) for mixed-use developments that provide public gathering spaces, entertainment attractions, and/or increased mobility options and connectivity.

Economic Development Recommendation #3: Maintain Hill Country aesthetic and office campus opportunities for employers within a more integrated land use pattern that better connects campuses to mixed-use destinations and mobility options.

Strategy 3.1 (Regulatory and Policy)

Encourage office campuses to orient buildings near surrounding retail/services and major intersections with parking and open spaces oriented away from major connection/access points.

Strategy 3.2 (Regulatory and Policy)

Explore the creation/expansion of design guidelines for developments along Highway 151 to support increased connectivity and orientation of buildings that support mixed-use or better integrated developments but still allow for flexibility to address security concerns.

Economic Development Recommendation #4: Provide opportunities for smaller scale, multi-tenant office and employment spaces and buildings to allow for a wider variety of types and sizes of businesses to locate in the plan area.

Strategy 4.1 (Partnerships)

Proactively interact with area businesses, developers, and property owners to identify opportunities for the development of multi-tenant office buildings by helping developers identify potential tenants for multi-tenant projects; encouraging large businesses and users to include additional rentable space within large campuses; and working to attract flexible workspace and co-working companies

|

to the plan area.

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7 Appendix: Maps, Figures, and Exhibits

Maps, Figures, and Exhibits referenced throughout this document are collected in the Appendix which begins on the next page.

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~~**Figure 13:** Focus Area #3 – Existing and Proposed Land Use/Massing~~

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Figure 2120: Streets for People and Places Map

Figure 2221: Amenities and Public Space Framework Map

Figure 2322: Conceptual Rendering of Leon Creek Greenway Expansion

Figure 2423: Housing Snapshot

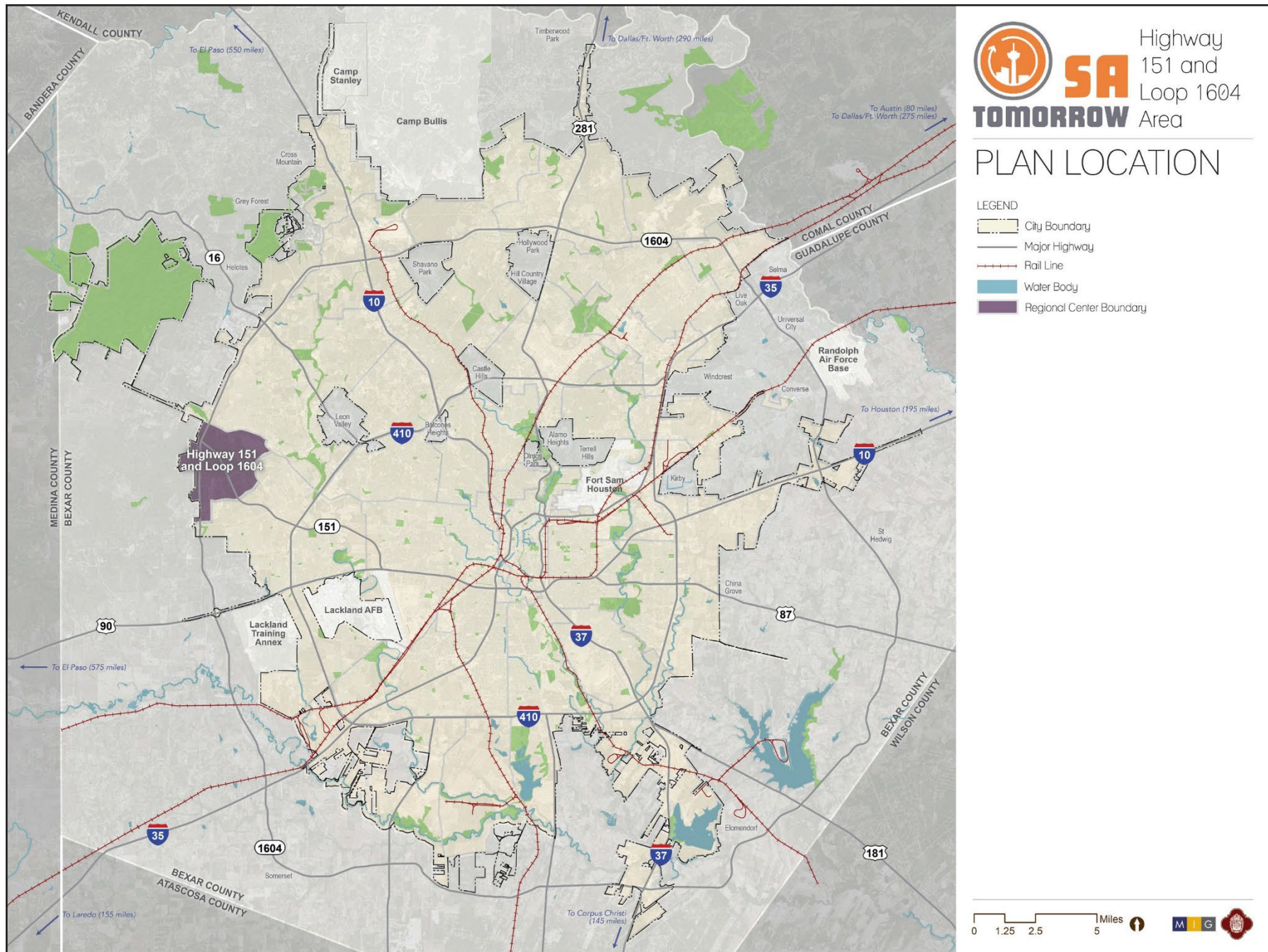
Figure 2524: Percent of Cost Burdened Households by Income and Tenure, 2018

Figure 2625: Planning Team Housing Challenges Analysis

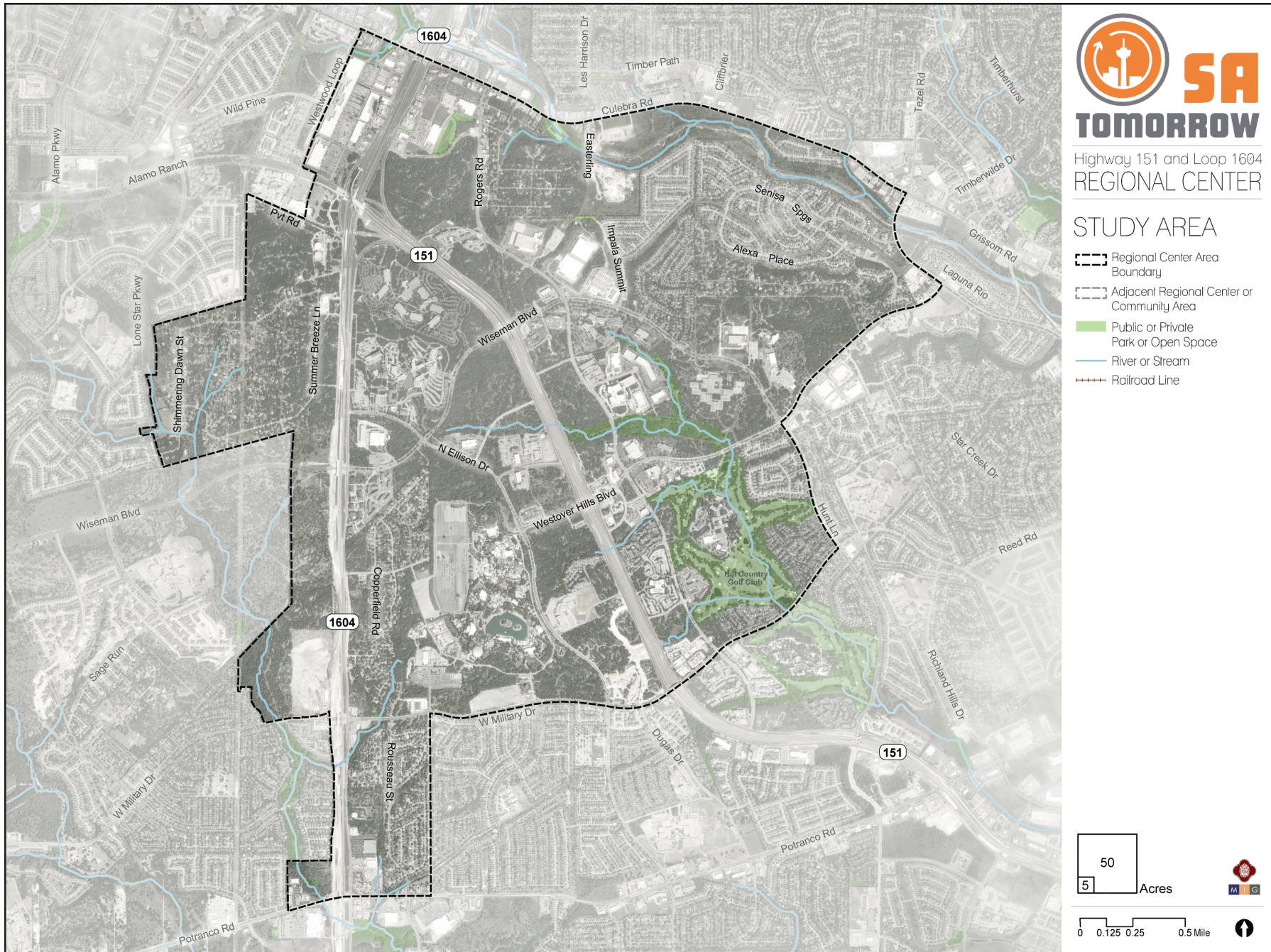
Figure 2726: Planning Team SWOT Analysis

Figure 2827: Attributes of a Vibrant Economic Place

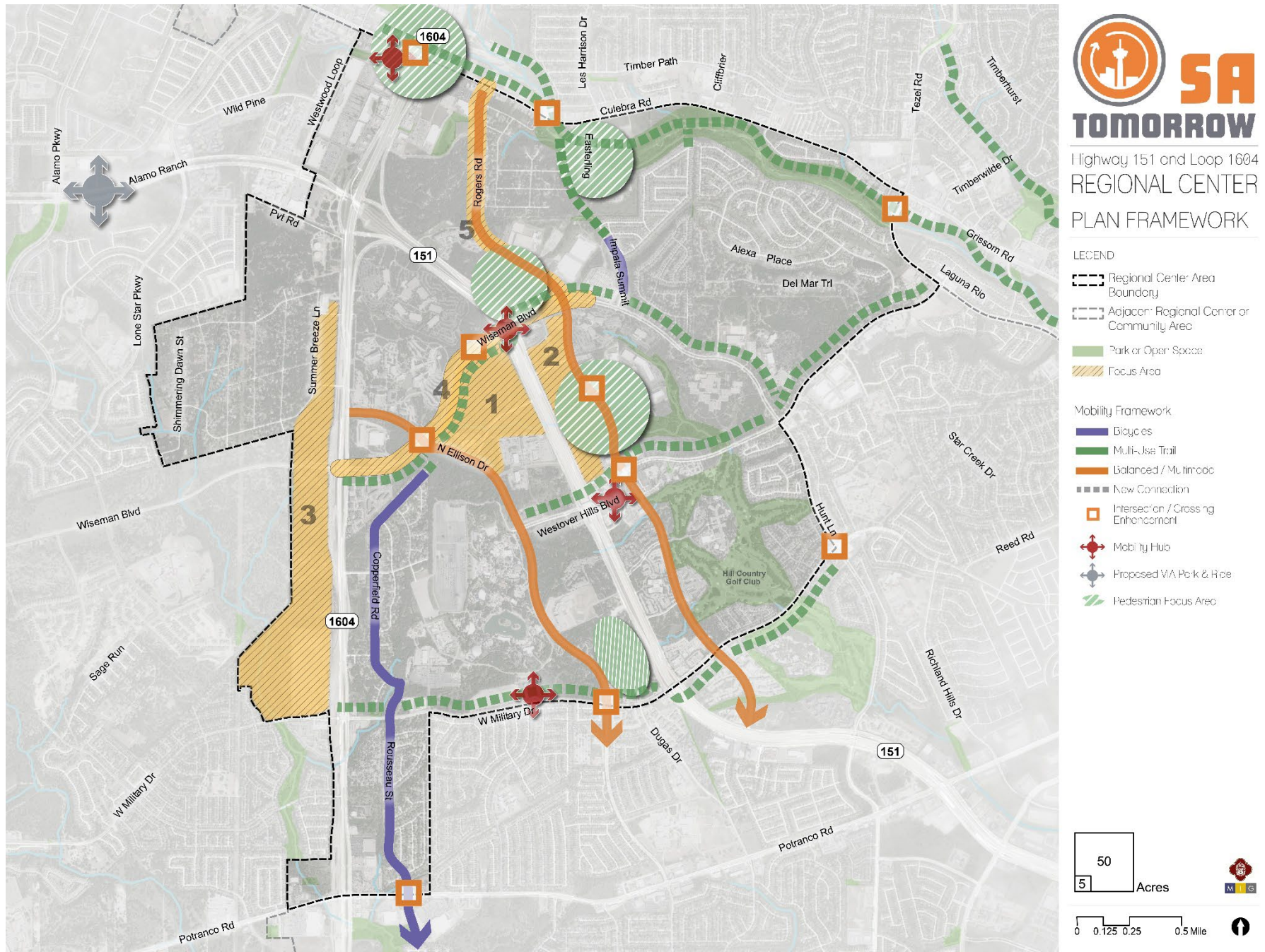
Exhibit 1: Existing Conditions Atlas



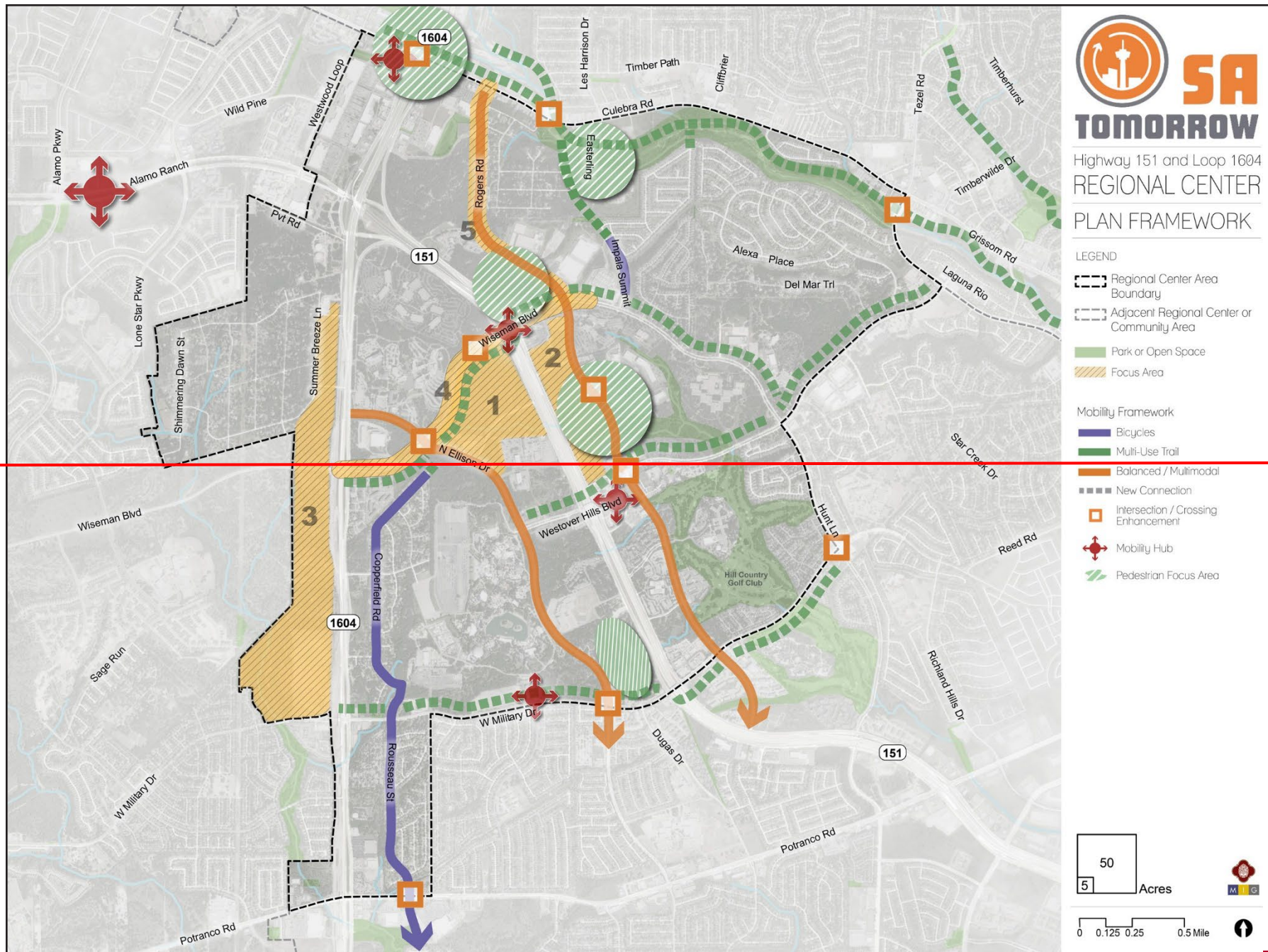
[Figure 1: Plan Location Map]



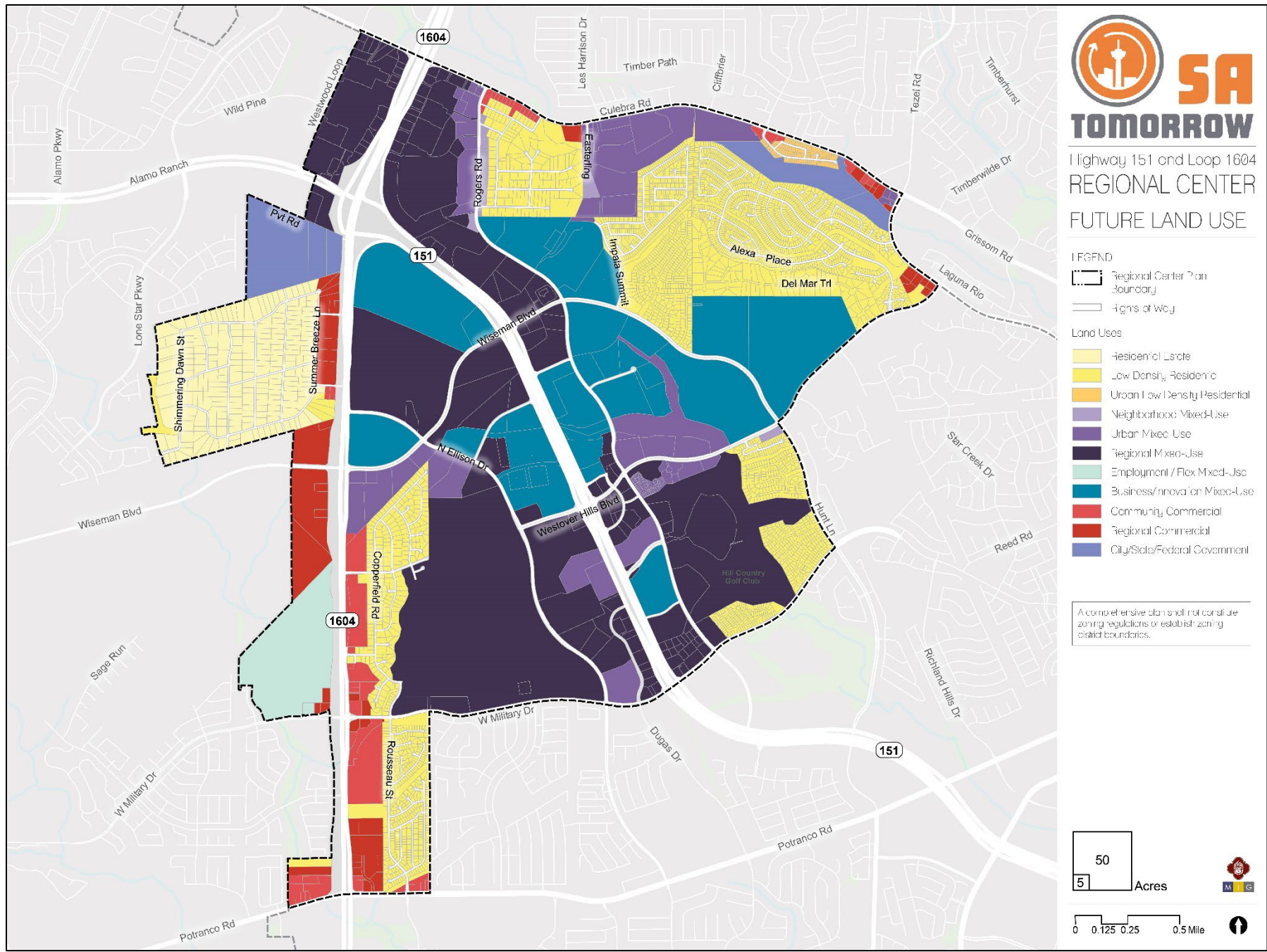
[Figure 2: Study Area Map]



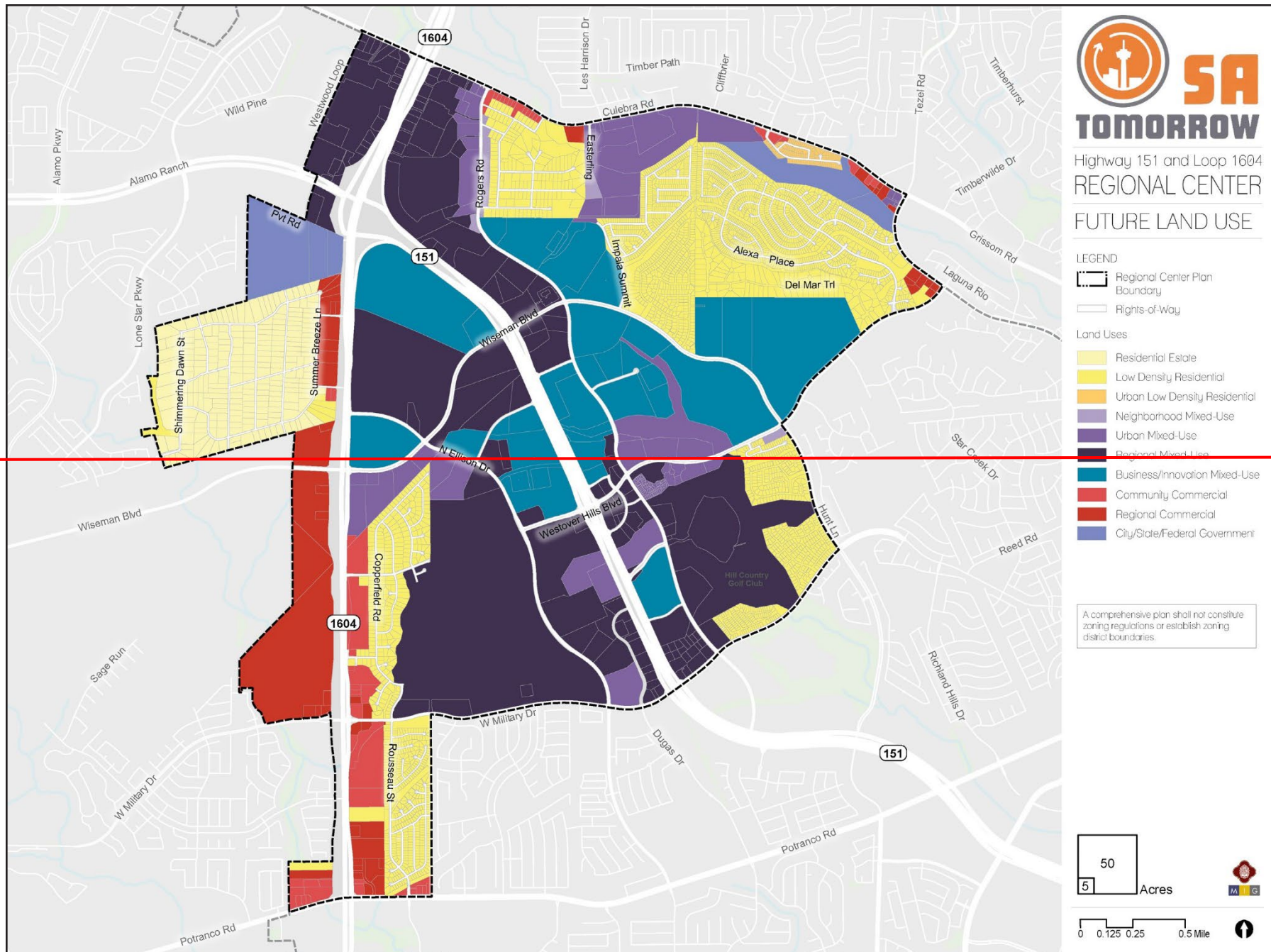
[Figure 3: Plan Framework Map]



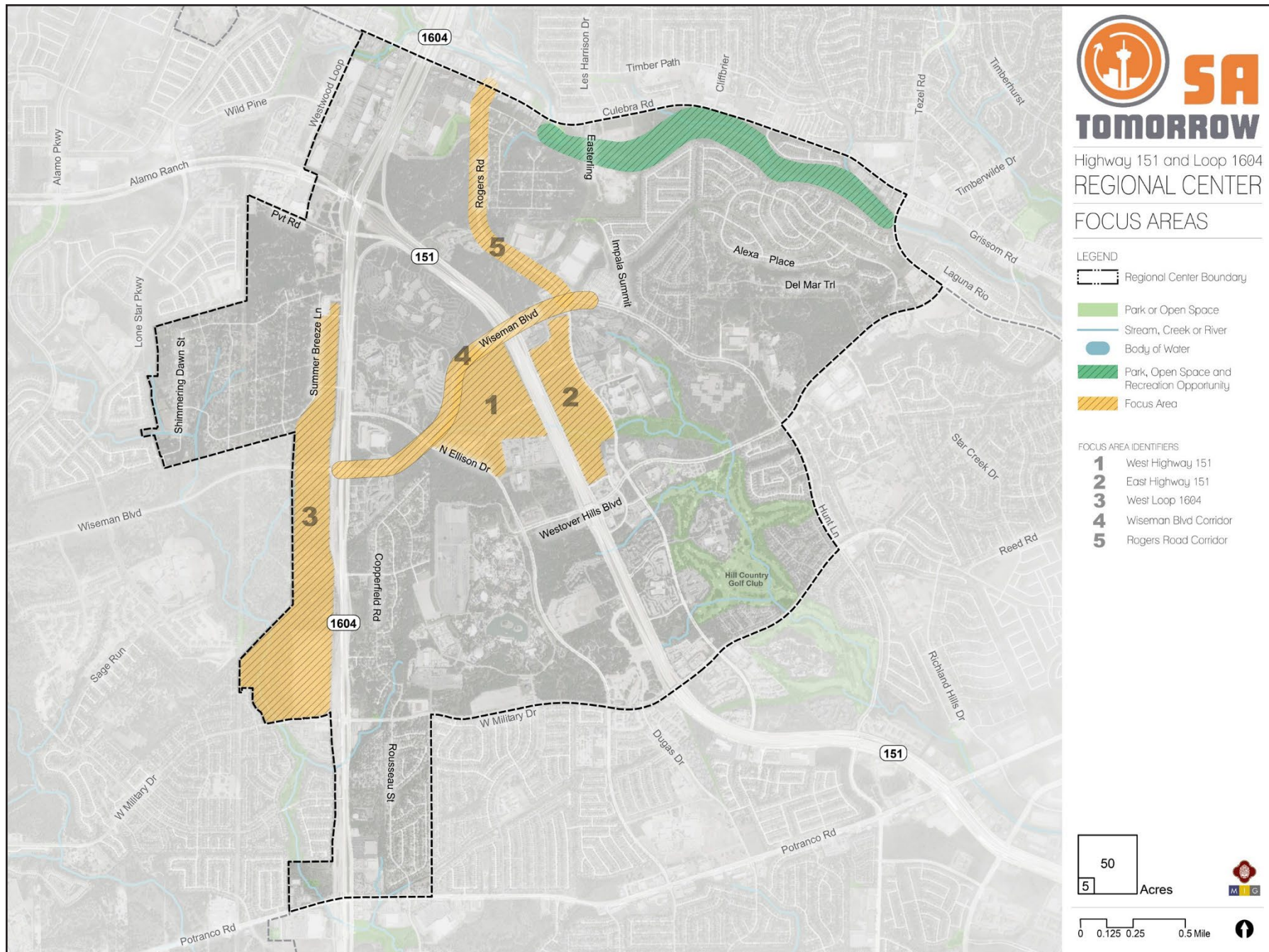
[Figure 3: Plan Framework Map]



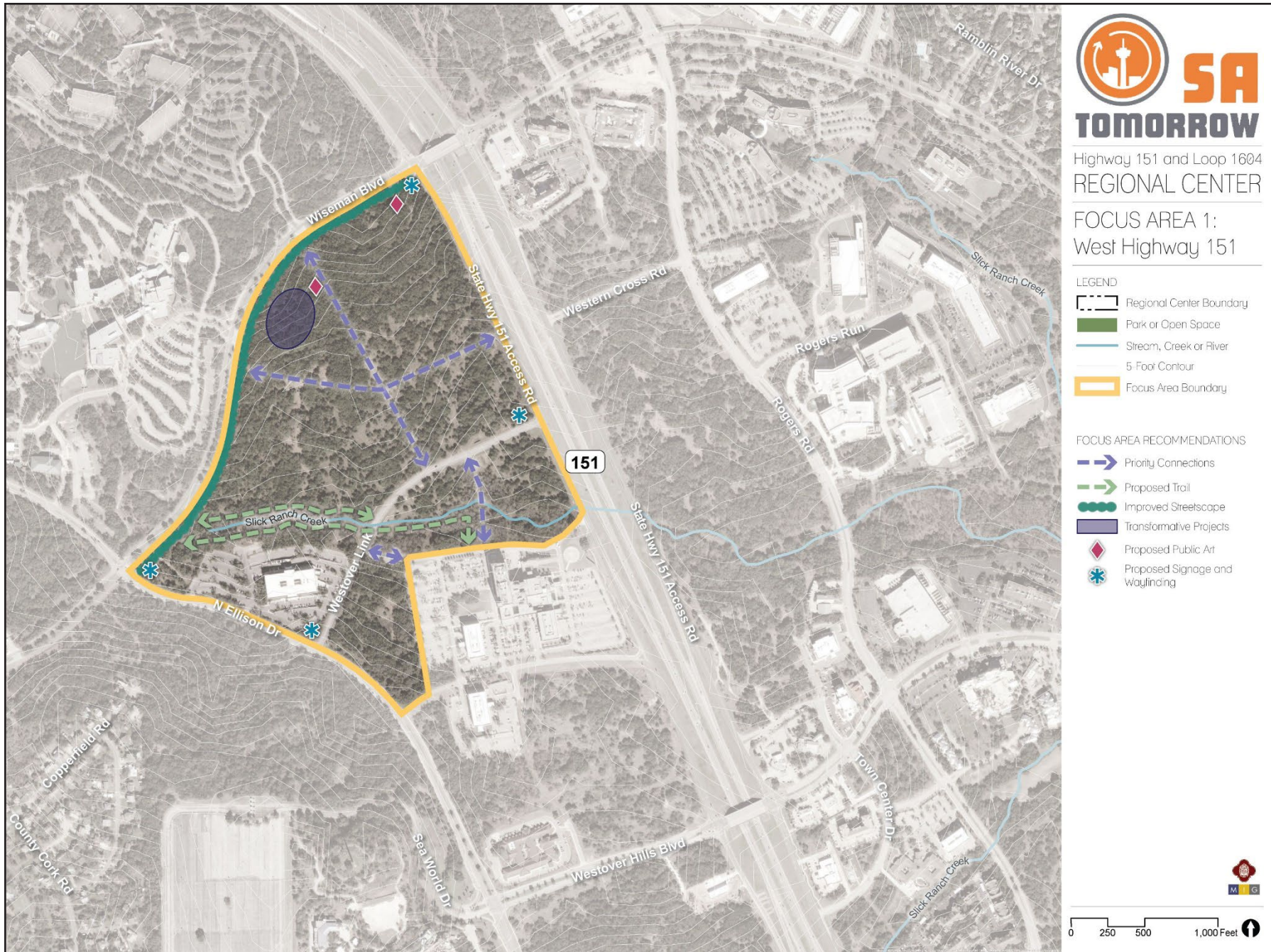
[Figure 4: Future Land Use Map]



[Figure 4: Future Land Use Map]



[Figure 5: Focus Area Framework Map]



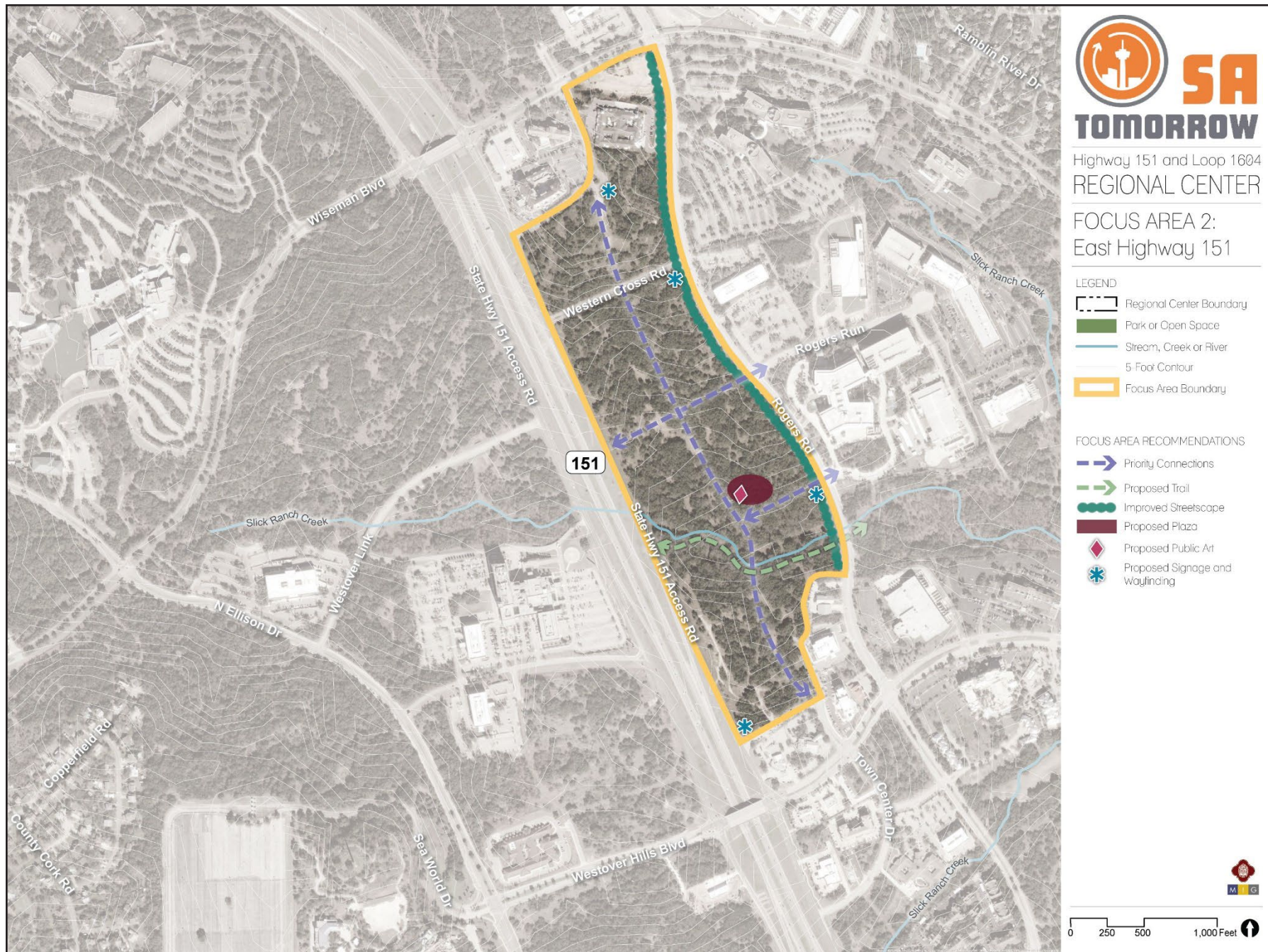
[Figure 6: Focus Area #1 Map – West Highway 151]



[Figure 7: Focus Area #1 - Existing]



[Figure 8: Focus Area #1 – Conceptual Rendering]



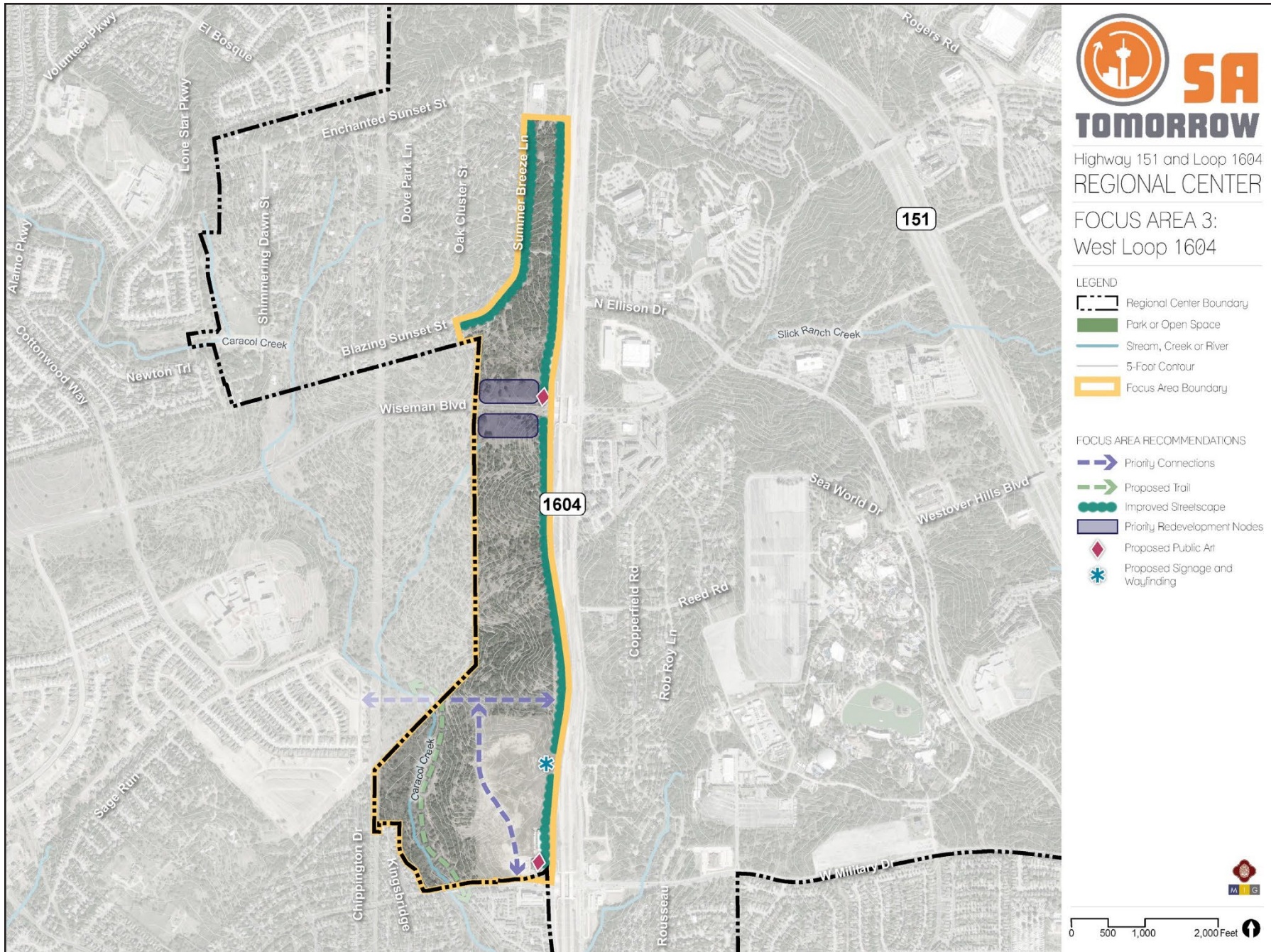
[Figure 9: Focus Area #2 Map – East Highway 151]



[Figure 10: Focus Area #2 – Existing]



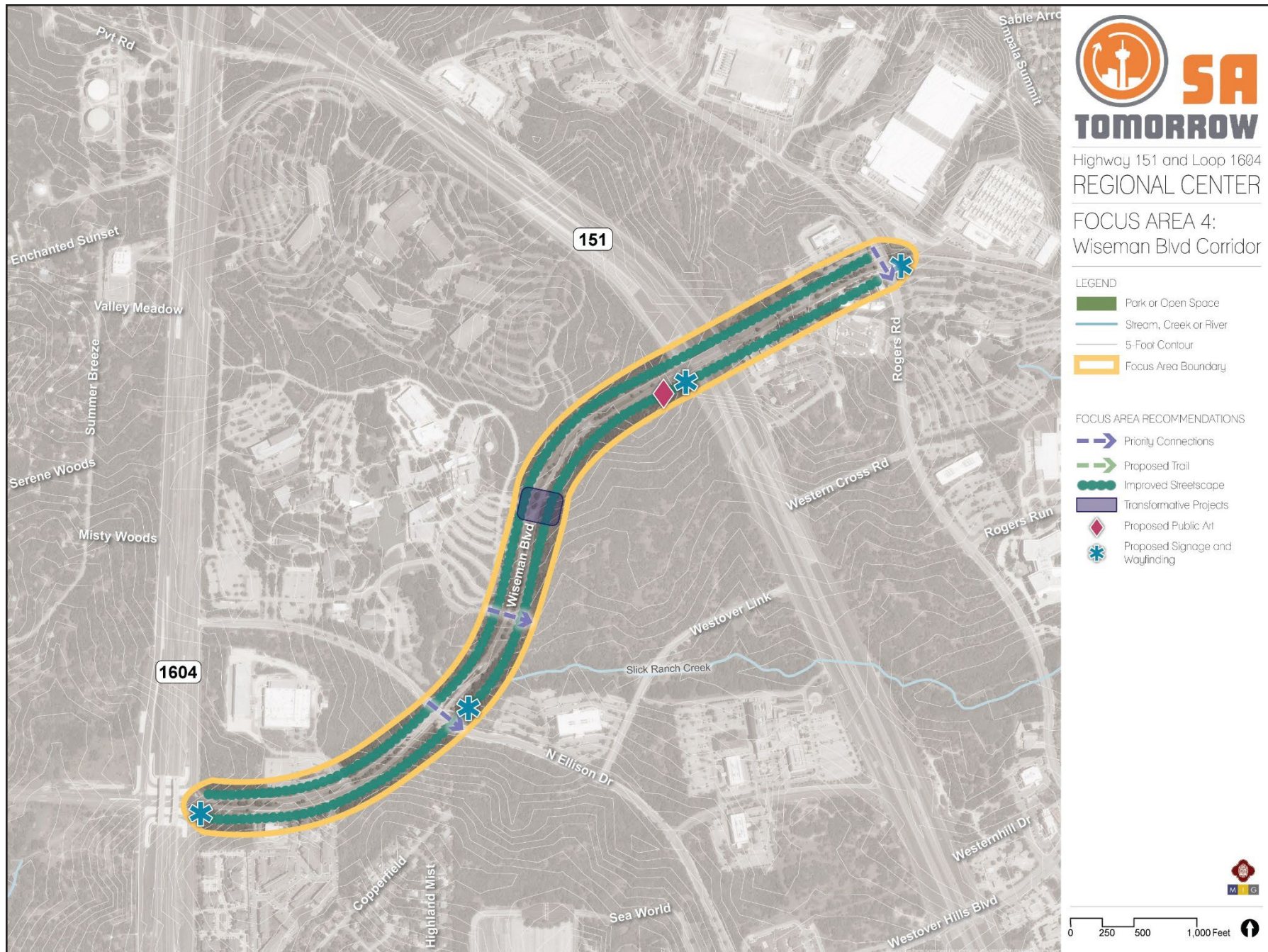
[Figure 11: Focus Area #2 – Conceptual Rendering]



[Figure 12: Focus Area #3 Map – West Loop 1604]



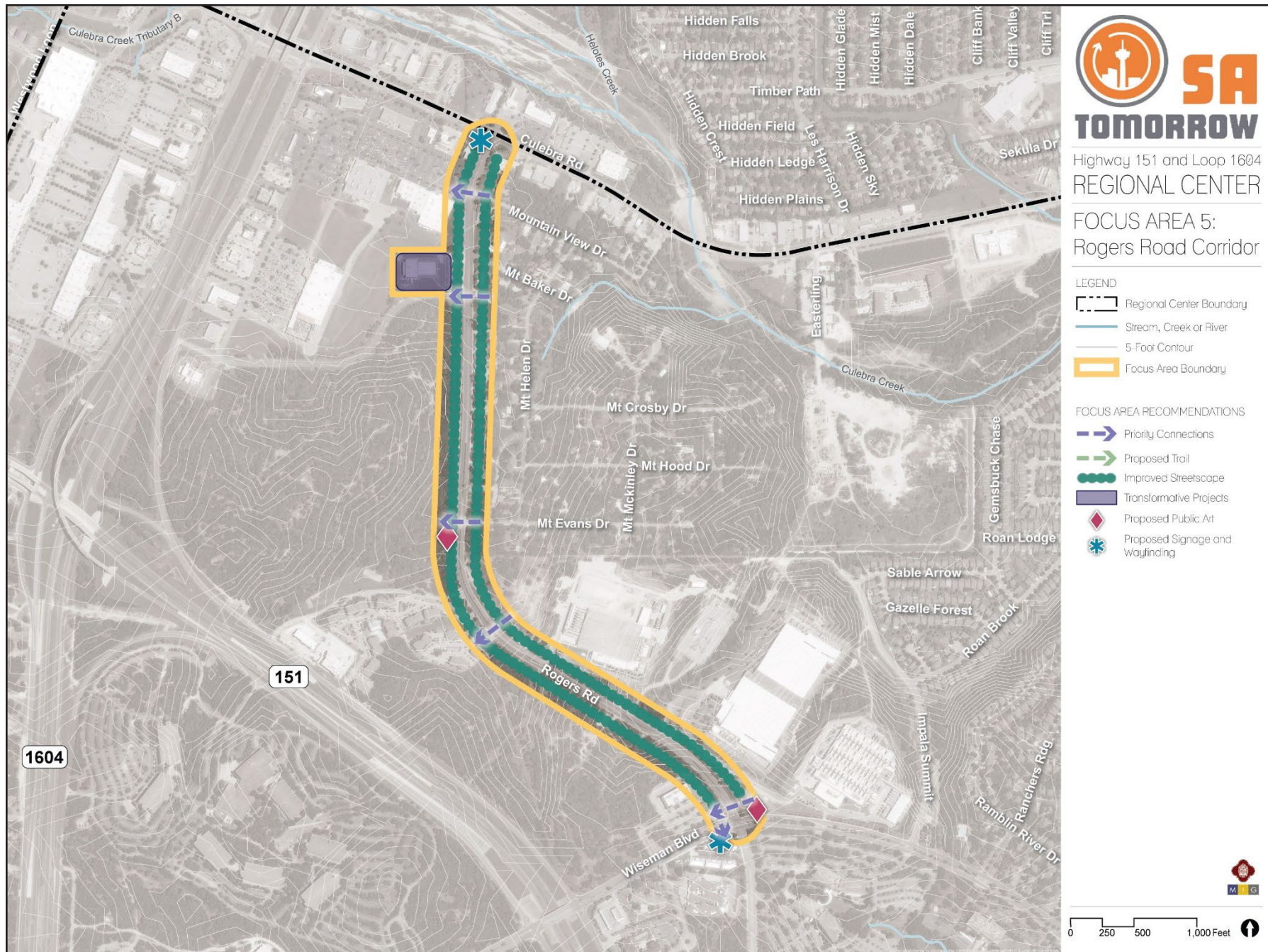
[Figure 13: Focus Area #3—Existing and Proposed Land Use/Massing]



[Figure 13-14: Focus Area #4 Map – Wiseman Boulevard Mixed-Use Corridor]



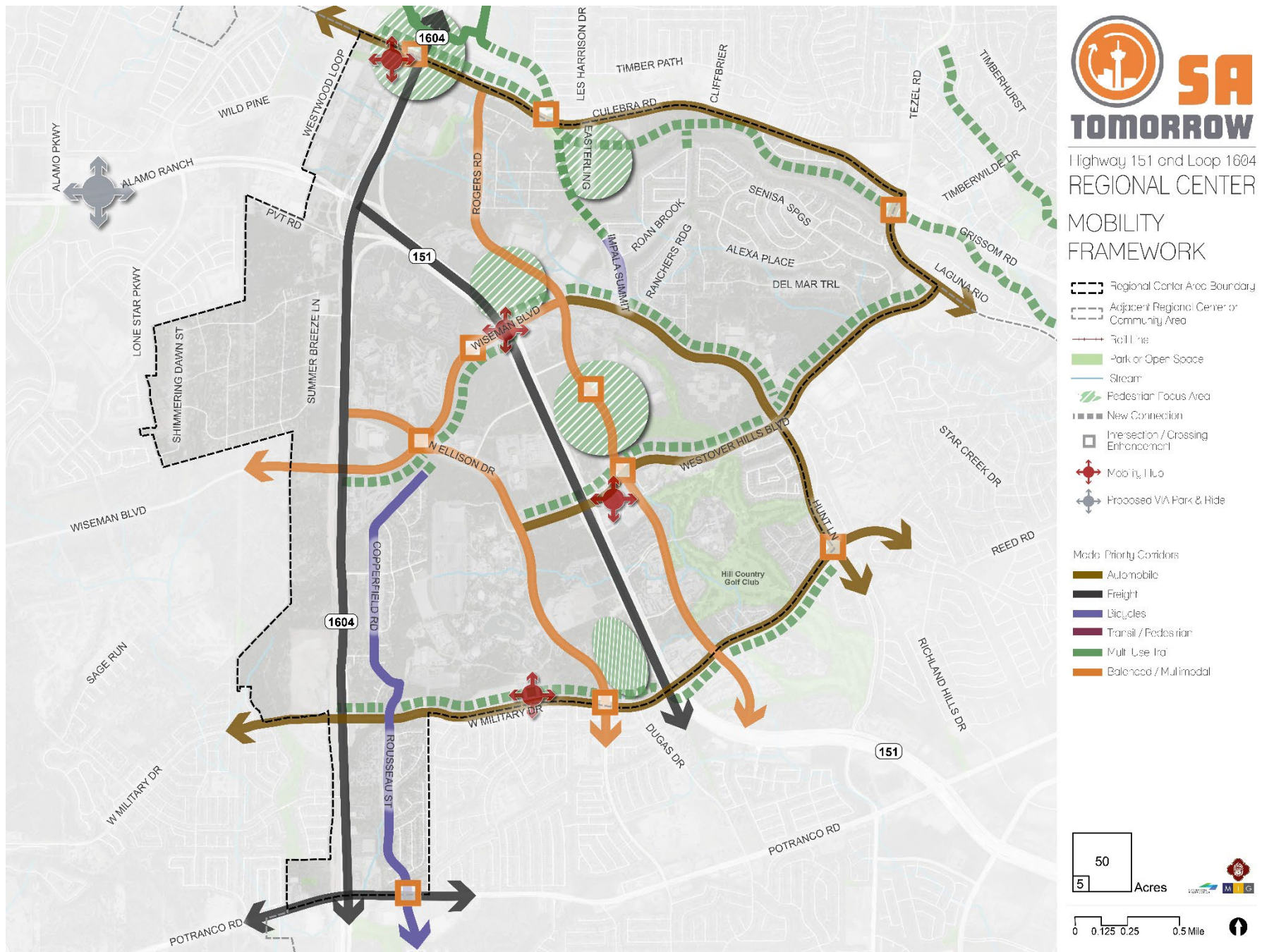
[Figure 1415: Focus Area #4 – Conceptual Rendering]



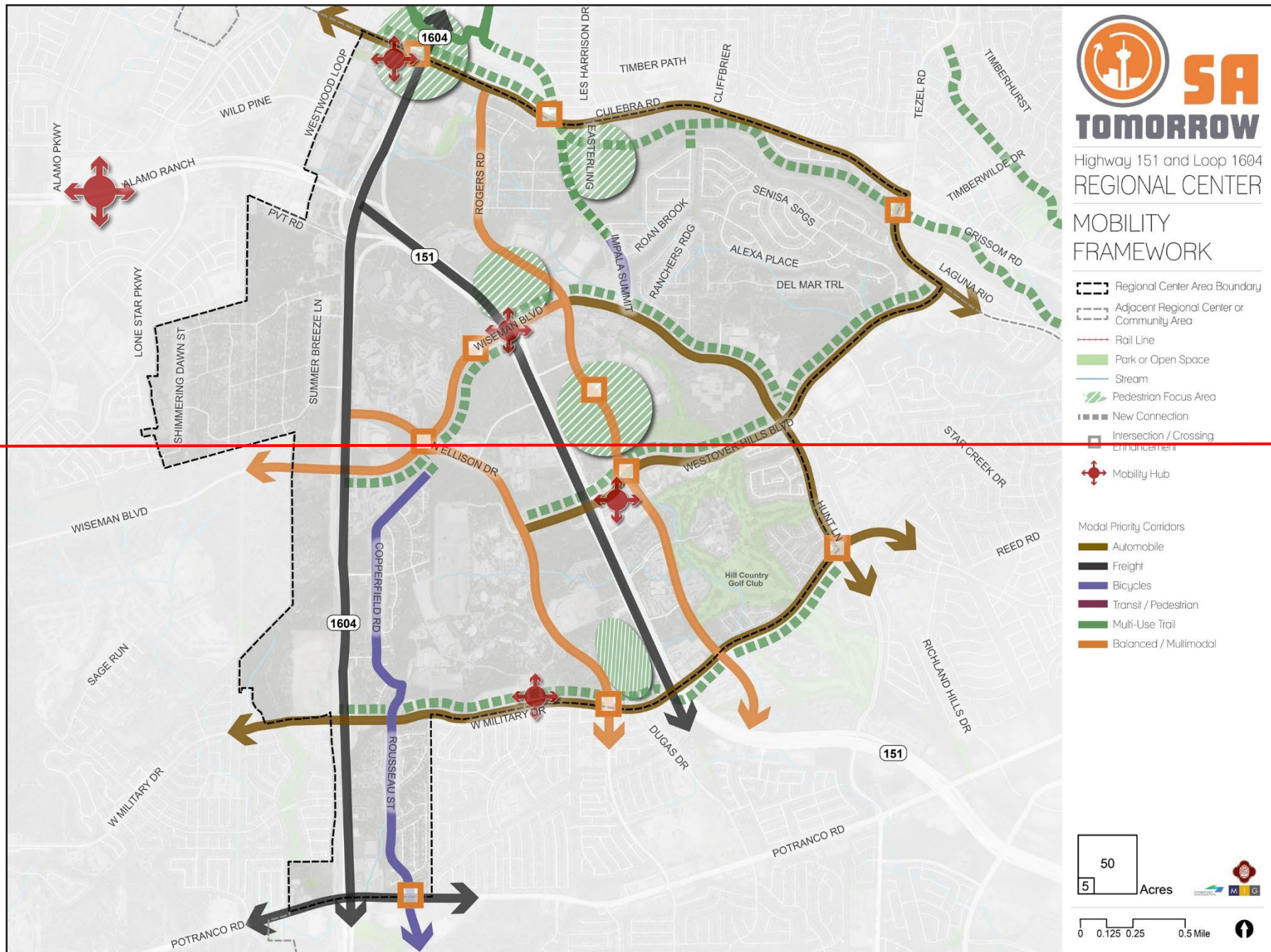
[Figure 15-16: Focus Area #5 Map – Rogers Road Mixed-Use Corridor]















[Figure 1617: Focus Area #5 – Conceptual Rendering]



[Figure 17-18: Mobility Framework Map]



[Figure 1718: Mobility Framework Map]

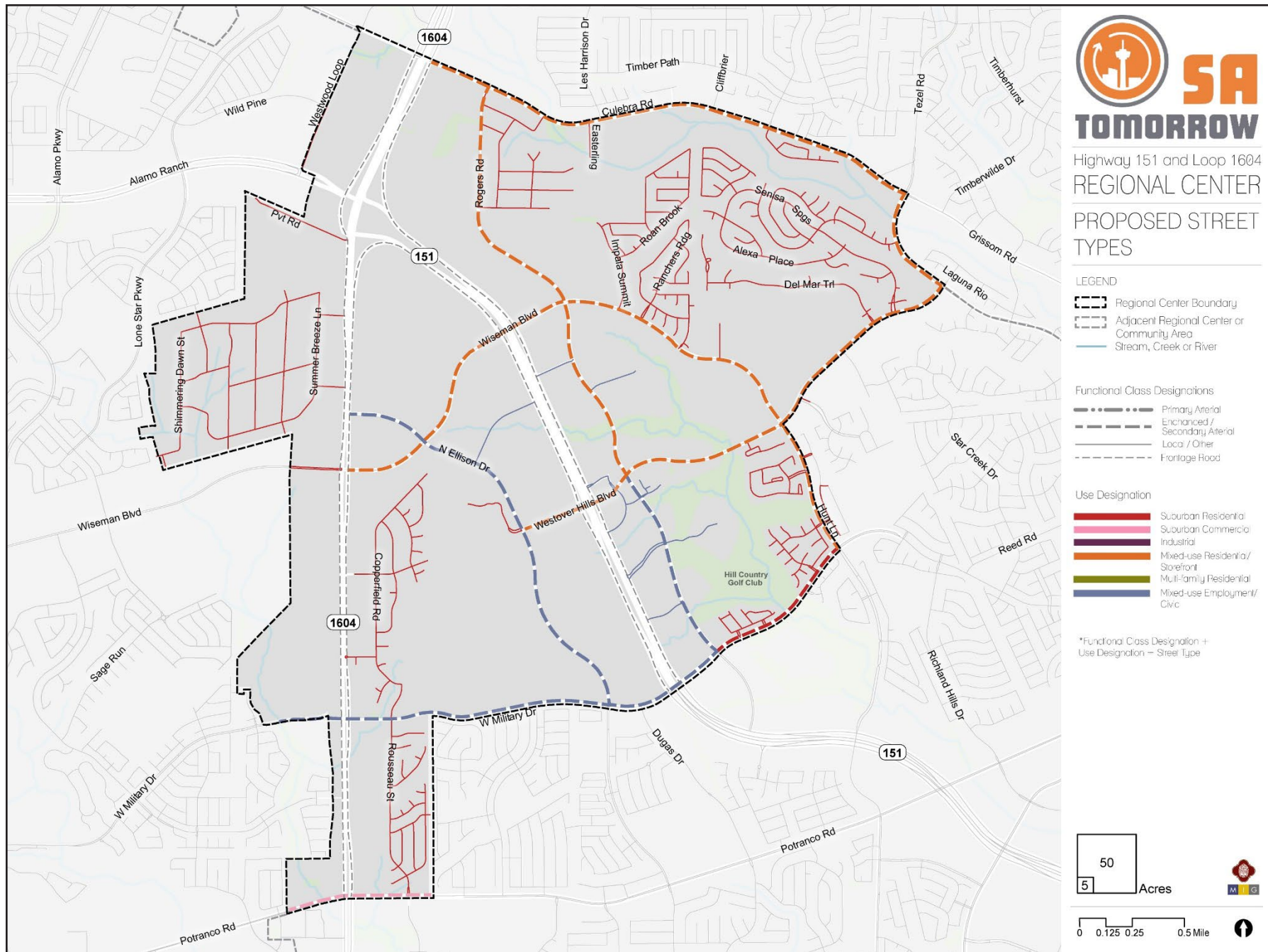
CONTEXT		 SUBURBAN RESIDENTIAL	 SUBURBAN COMMERCIAL	 INDUSTRIAL	 MULTI-FAMILY RESIDENTIAL	 MIXED-USE RESIDENTIAL/STOREFRONT	 MIXED-USE EMPLOYMENT/CIVIC	 VARIABLE
FUNCTIONAL CLASSIFICATION	 SUPER ARTERIAL	NA	Suburban Commercial Super Arterial	Industrial Super Arterial	NA	NA	Mixed-Use Employment/Civic Super Arterial	Depends on Specific Context
	 PRIMARY ARTERIAL	Suburban Residential Primary Arterial	Suburban Commercial Primary Arterial	Industrial Primary Arterial	Urban/Suburban Multi-family Primary Arterial	Mixed-Use Residential/Storefront Primary Arterial	Mixed-Use Employment/Civic Primary Arterial	
	 ENHANCED/SECONDARY ARTERIAL	Suburban Residential Secondary Arterial	Suburban Commercial Secondary Arterial	Industrial Secondary Arterial	Urban/Suburban Multi-family Secondary Arterial	Mixed-Use Residential/Storefront Secondary Arterial	Mixed-Use Employment/Civic Secondary Arterial	
	 RURAL	Rural Residential	Rural Commercial	Industrial Rural	NA	NA	NA	
	 LOCAL/OTHER	Suburban Local		Industrial Local	Urban/Suburban Local	Mixed-Use Local		

Moving Vehicles

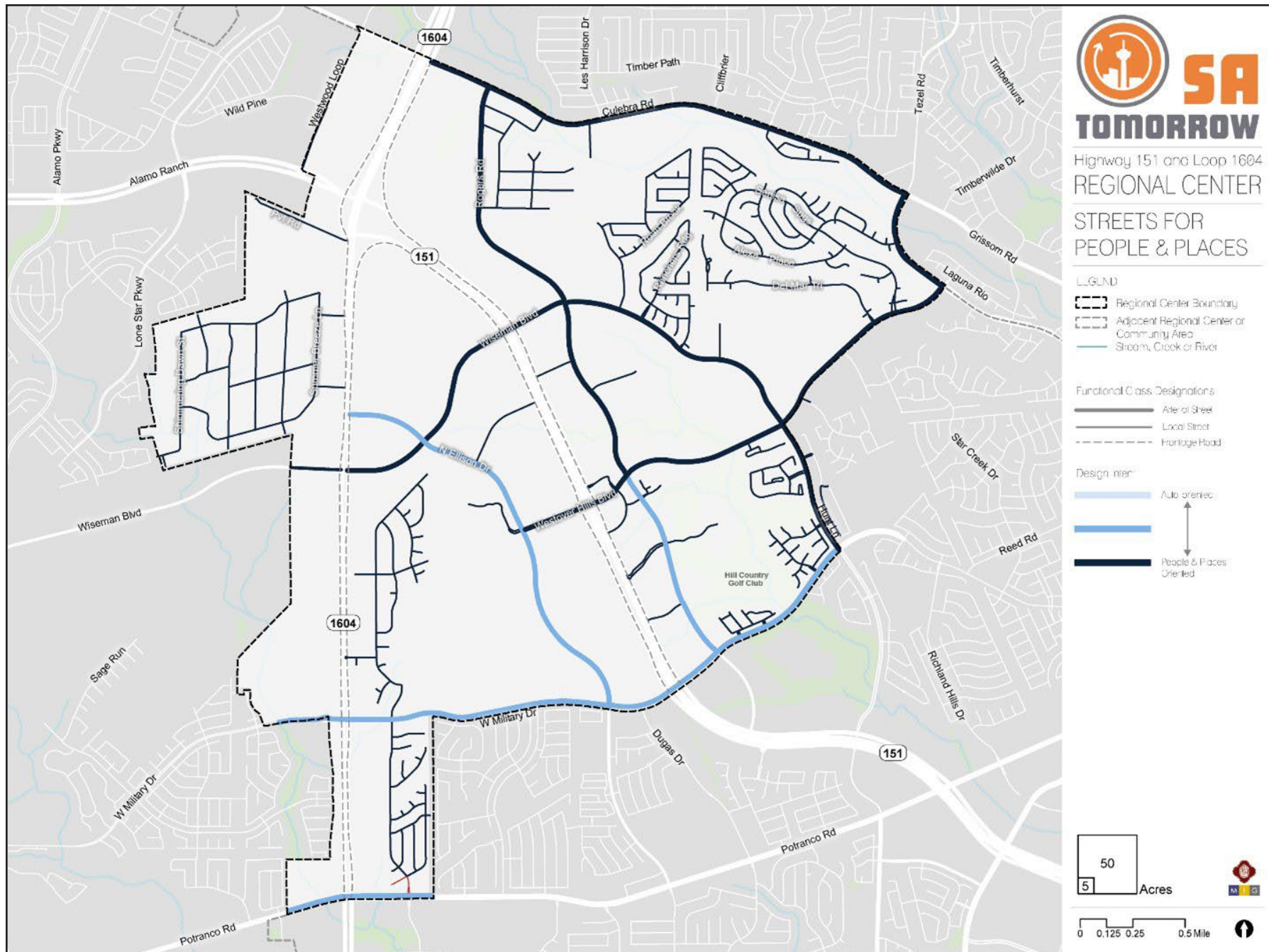
Emphasis

People and Places

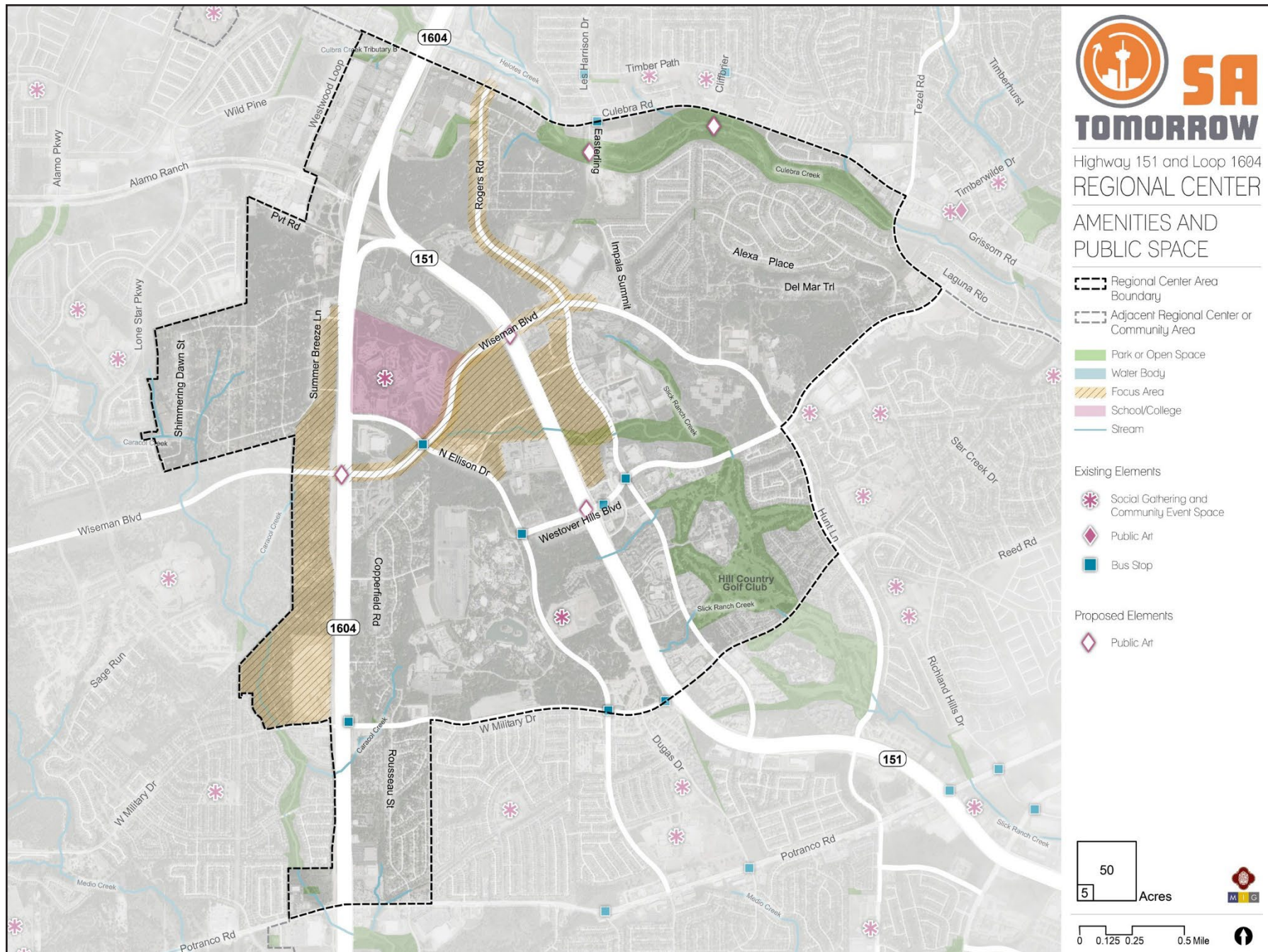
[Figure 18-19: Street Types: Functional Classification and Land Use Context Matrix]



[Figure 1920: Street Types Map]



[Figure 2021: Streets for People and Places Map]



[Figure 2122: Amenities and Public Space Framework Map]



[Figure 2223: Comceptual Rendering of Leon Creek Greenway Expansion]

HOUSING SNAPSHOT

Total Population |
13,542

Total Households |
4,871

Annual Household
Growth, 2010-2018 |
4.9% (COSA | 1.2%)

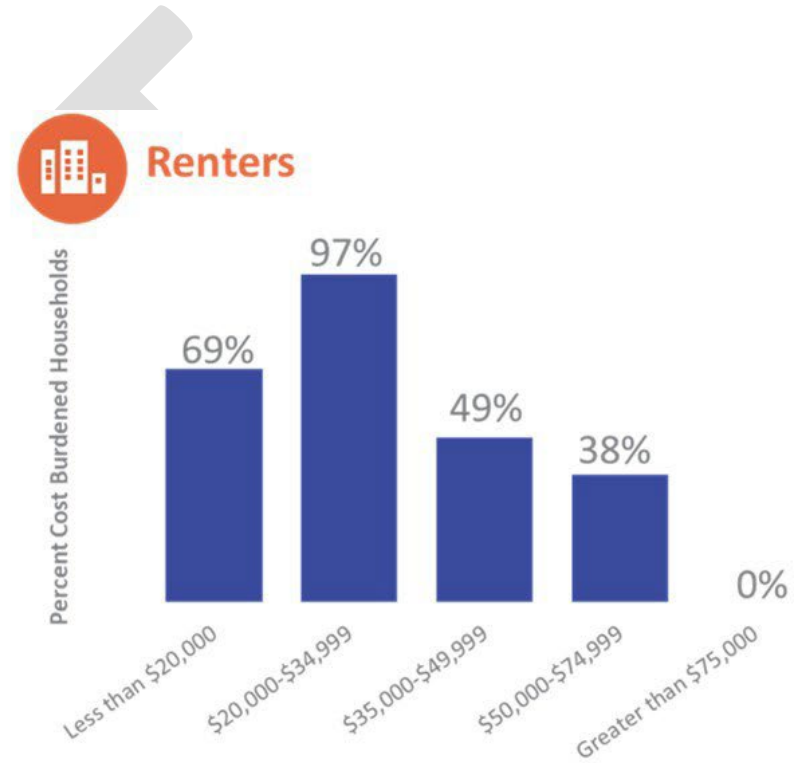
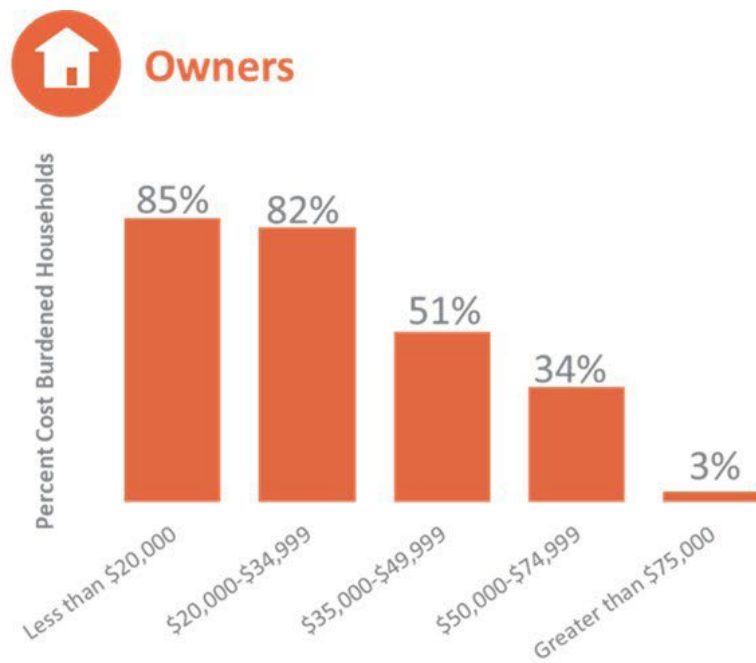
Average Household Size
| 2.78 persons (COSA |
2.71)

One-Person Households
| 22%
(COSA | 29%)

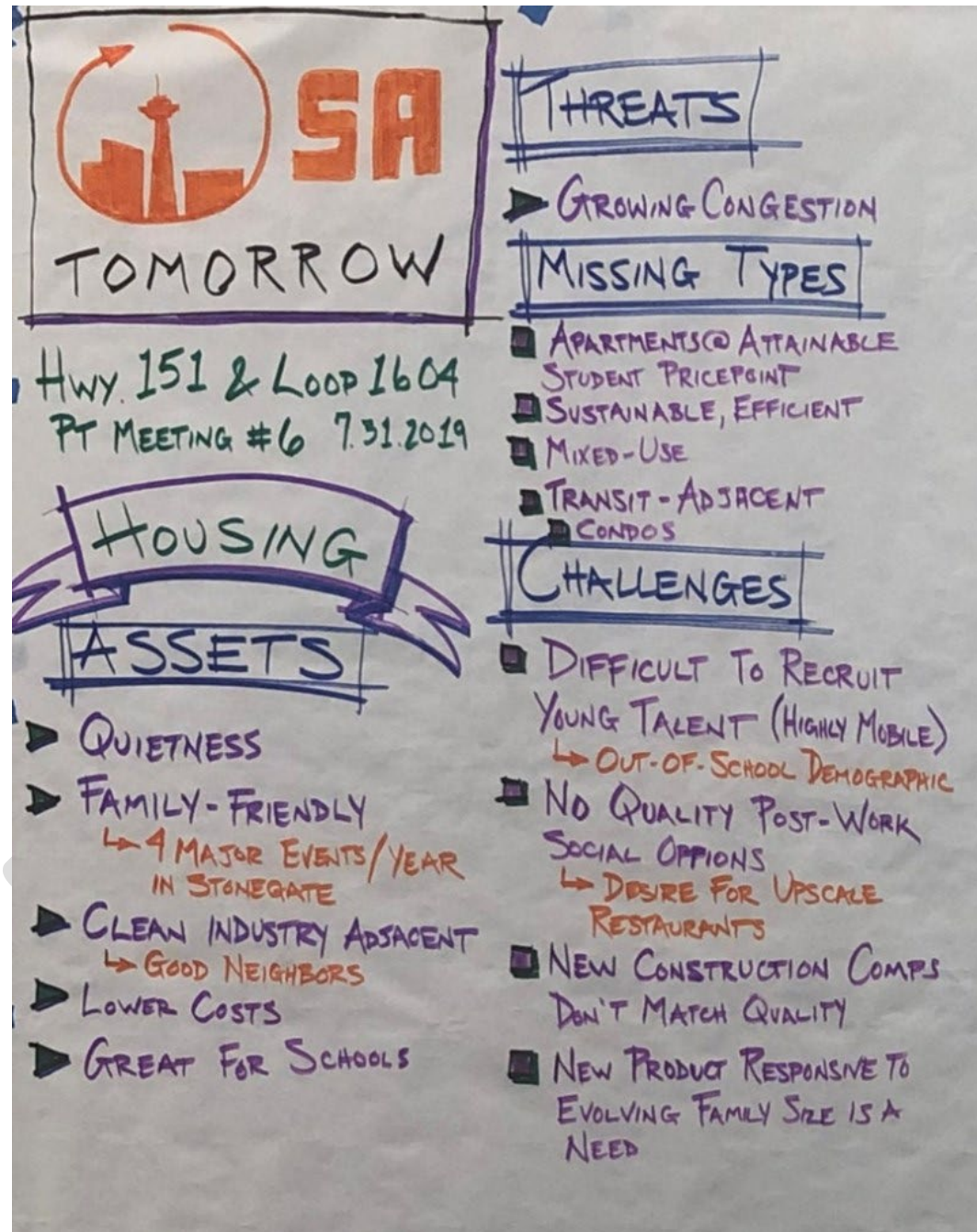
Non-Family Households
| 27%
(COSA | 35%)

Average Household
Income | \$99,500
(COSA | \$70,019)

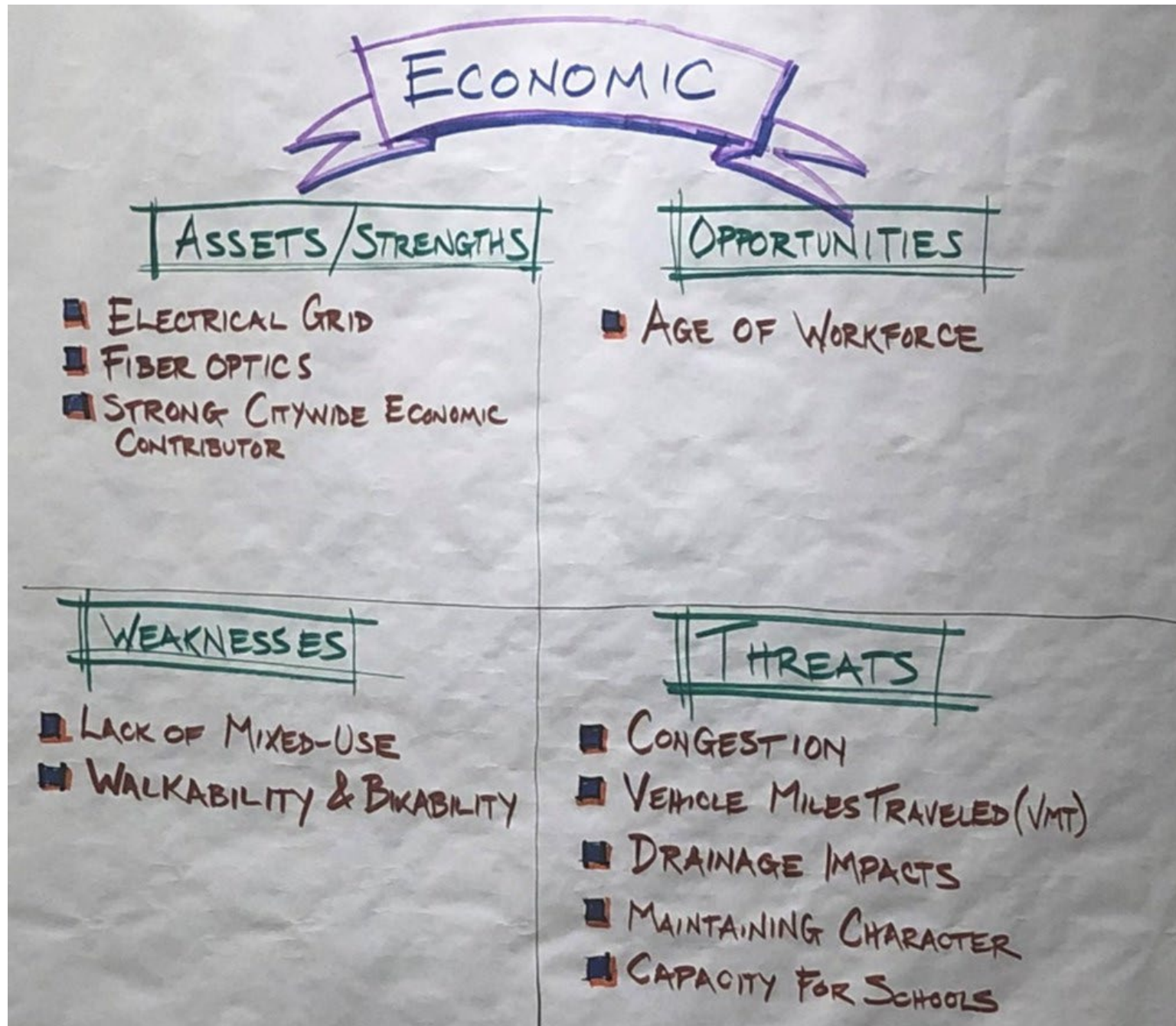
[Figure 2324: Housing Snapshot]



[Figure 24-25: Percent of Cost Burdened Households by Income and Tenure, 2018]



[Figure 2526: Planning Team Housing Challenges Analysis]



[Figure 2627: Planning Team SWOT Analysis]

BRAND

Brands are important to establish an identity, create cohesion, and market employment places. This is especially important for newer or less well-established places

ORGANIZATIONS/PARTNERSHIPS

Non-profit and public-private agencies support the creation and continued operation of employment places. These agencies include BIDs and Economic Development Corporations

ANCHOR INSTITUTIONS

Anchor institutions, such as universities and hospitals, need to be connected and integrated into the fabric and strategy of the place

URBAN DESIGN/PLANNING

Rezoning and design efforts allow for a more vibrant mix of land uses. Parks and transportation also help catalyze these places

TRANSPORTATION

Transit connectivity has been identified as one of the most important factors in the location decisions of firms and start-ups and is critical to the success of employment places

INVESTMENTS

Investments in parks and other amenities are important catalysts to help create identity and a sense of place in employment places

FINANCE/INCENTIVES

Improvement district tools like BIDs and TIF can help to finance capital projects as well as the operation and maintenance of the place

[Figure 2728: Attributes of a Vibrant Economic Place]