

## HISTORIC AND DESIGN REVIEW COMMISSION

March 02, 2022

**HDRC CASE NO:** 2022-090  
**ADDRESS:** 100 & 200 Blocks of Greenlawn  
**LEGAL DESCRIPTION:** ROW in 100 & 200 blocks of Greenlawn  
**ZONING:** H  
**CITY COUNCIL DIST.:** 1  
**DISTRICT:** Greenlawn Estates Historic District  
**APPLICANT:** Luis Ocura/City of San Antonio  
**OWNER:** City of San Antonio  
**TYPE OF WORK:** Sidewalk installation  
**APPLICATION RECEIVED:** January 28, 2022  
**60-DAY REVIEW:** Not applicable due to City Council Emergency Orders  
**CASE MANAGER:** Hannah Leighner

### REQUEST:

The applicant is requesting a Certificate of Appropriateness for approval to install new sidewalks and driveway approaches on the north side of Greenlawn Dr connecting the existing sidewalk at 5616 Frontage Rd to Vance Jackson Rd, and to install new ADA ramps to connect to the existing sidewalks on either side of Greenlawn Dr at the intersection with Vance Jackson Rd.

### APPLICABLE CITATIONS:

#### 5. Sidewalks, Walkways, Driveways, and Curbing

##### A. SIDEWALKS AND WALKWAYS

- i. *Maintenance*—Repair minor cracking, settling, or jamming along sidewalks to prevent uneven surfaces. Retain and repair historic sidewalk and walkway paving materials—often brick or concrete—in place.
- ii. *Replacement materials*—Replace those portions of sidewalks or walkways that are deteriorated beyond repair. Every effort should be made to match existing sidewalk color and material.
- iii. *Width and alignment*—Follow the historic alignment, configuration, and width of sidewalks and walkways. Alter the historic width or alignment only where absolutely necessary to accommodate the preservation of a significant tree.
- iv. *Stamped concrete*—Preserve stamped street names, business insignias, or other historic elements of sidewalks and walkways when replacement is necessary.
- v. *ADA compliance*—Limit removal of historic sidewalk materials to the immediate intersection when ramps are added to address ADA requirements.

##### B. DRIVEWAYS

- i. *Driveway configuration*—Retain and repair in place historic driveway configurations, such as ribbon drives. Incorporate a similar driveway configuration—materials, width, and design—to that historically found on the site. Historic driveways are typically no wider than 10 feet. Pervious paving surfaces may be considered where replacement is necessary to increase stormwater infiltration.
- ii. *Curb cuts and ramps*—Maintain the width and configuration of original curb cuts when replacing historic driveways. Avoid introducing new curb cuts where not historically found.

##### C. CURBING

- i. *Historic curbing*—Retain historic curbing wherever possible. Historic curbing in San Antonio is typically constructed of concrete with a curved or angular profile.
- ii. *Replacement curbing*—Replace curbing in-kind when deteriorated beyond repair. Where in-kind replacement is not be feasible, use a comparable substitute that duplicates the color, texture, durability, and profile of the original. Retaining walls and curbing should not be added to the sidewalk design unless absolutely necessary.

#### 6. Non-Residential and Mixed Use Streetscapes

##### A. STREET FURNITURE

- i. *Historic street furniture*—Preserve historic site furnishings, including benches, lighting, tree grates, and other features.
- ii. *New furniture*—Use street furniture such as benches, trash receptors, tree grates, and tables that are simple in design and are compatible with the style and scale of adjacent buildings and outdoor spaces when historic furnishings do not exist.

## B. STREET TREES

i. *Street trees*—Protect and maintain existing street trees. Replace damaged or dead trees with trees of a similar species, size, and growth habit.

## C. PAVING

i. *Maintenance and alterations*—Repair stone, masonry, or glass block pavers using in-kind materials whenever possible. Utilize similar materials that are compatible with the original in terms of composition, texture, color, and detail, when in-kind replacement is not possible.

## D. LIGHTING

i. *General*—See UDC Section 35-392 for detailed lighting standards (height, shielding, illumination of uses, etc.).

ii. *Maintenance and alterations*—Preserve historic street lights in place and maintain through regular cleaning and repair as needed.

iii. *Pedestrian lighting*—Use appropriately scaled lighting for pedestrian walkways, such as short poles or light posts (bollards).

iv. *Shielding*—Direct light downward and shield light fixtures using cut-off shields to limit light spill onto adjacent properties.

v. *Safety lighting*—Install motion sensors that turn lights on and off automatically when safety or security is a concern. Locate these lighting fixtures as discreetly as possible on historic structures and avoid adding more fixtures than necessary.

## 7. Off-Street Parking

### A. LOCATION

i. *Preferred location*—Place parking areas for non-residential and mixed-use structures at the rear of the site, behind primary structures to hide them from the public right-of-way. On corner lots, place parking areas behind the primary structure and set them back as far as possible from the side streets. Parking areas to the side of the primary structure are acceptable when location behind the structure is not feasible. See UDC Section 35-310 for district-specific standards.

ii. *Front*—Do not add off-street parking areas within the front yard setback as to not disrupt the continuity of the streetscape.

iii. *Access*—Design off-street parking areas to be accessed from alleys or secondary streets rather than from principal streets whenever possible.

### B. DESIGN

i. *Screening*—Screen off-street parking areas with a landscape buffer, wall, or ornamental fence two to four feet high—or a combination of these methods. Landscape buffers are preferred due to their ability to absorb carbon dioxide. See UDC Section 35-510 for buffer requirements.

ii. *Materials*—Use permeable parking surfaces when possible to reduce run-off and flooding. See UDC Section 35-526(j) for specific standards.

iii. *Parking structures*—Design new parking structures to be similar in scale, materials, and rhythm of the surrounding historic district when new parking structures are necessary.

## 8. Americans with Disabilities Act (ADA) Compliance

### A. HISTORIC FEATURES

i. *Avoid damage*—Minimize the damage to the historic character and materials of the building and sidewalk while complying with all aspects of accessibility requirements.

ii. *Doors and door openings*—Avoid modifying historic doors or door openings that do not conform to the building and/or accessibility codes, particularly on the front façade. Consider using a discretely located addition as a means of providing accessibility.

### B. ENTRANCES

i. *Grade changes*—Incorporate minor changes in grade to modify sidewalk or walkway elevation to provide an accessible entry when possible.

ii. *Residential entrances*—The preferred location of new ramps is at the side or rear of the building when convenient for the user.

iii. *Non-residential and mixed use entrances*—Provide an accessible entrance located as close to the primary entrance as possible when access to the front door is not feasible.

### C. DESIGN

i. *Materials*—Design ramps and lifts to compliment the historic character of the building and be visually unobtrusive as to minimize the visual impact, especially when visible from the public right-of-way.

- ii. *Screening*—Screen ramps, lifts, or other elements related to ADA compliance using appropriate landscape materials. Refer to Guidelines for Site Elements for additional guidance.
- iii. *Curb cuts*—Install new ADA curb cuts on historic sidewalks to be consistent with the existing sidewalk color and texture while minimizing damage to the historical sidewalk.

## **FINDINGS:**

- a. The 100-200 Block of Greenlawn Drive runs east to west through the length of Greenlawn Estates Historic District. The project is bound by Vance Jackson Rd and the I10 Frontage Road. The streetscape features historic curbing and does not currently feature sidewalks. The COSA sidewalk project has proposed to install sidewalks and driveway approaches.
- b. **SIDEWALK INSTALLATION** – The applicant has proposed to install sidewalks on the north side of Greenlawn Drive between Vance Jackson Rd and the Frontage Rd at I10. The applicant has specified that the new sidewalks will be 4’ wide and will extend from the existing curb. Guideline 5.A.iii for Site Elements states that the installation of new sidewalks should follow the historic alignment, configuration, and width of existing sidewalks and walkways. The Guidelines note that the historic width or alignment of sidewalks should be altered only where absolutely necessary to accommodate the preservation of a significant tree. Additionally, Guideline 5.C.i for Site Elements states that historic curbing should be retained wherever possible. Historic curbing in San Antonio is typically constructed of concrete with a curved or angular profile. Staff finds the proposal to be appropriate. Sidewalks located in historic districts typically feature planted medians; staff finds the setback and placement of the proposed sidewalk installation along the street curb to be appropriate as this provides minimal disruption to existing water meters, mature trees, and existing fences, which would need to be removed or relocated should a planted median be incorporated.
- c. **DRIVEWAY APPROACH INSTALLATION** – The applicant has proposed to install new driveway approaches along the project area. The Historic Design Guidelines for Site Elements state that the width and configuration of original curb cuts should be maintained when replacing historic driveways. The introduction of new curb cuts where not historically found should be avoided. Staff finds that any modifications to existing driveway approach widths or material will require additional review.
- d. **ADA RAMP INSTALLATION** – The applicant has proposed to install ADA ramps at the north and south corners of Vance Jackson Rd and Greenlawn Drive. Guideline 8.C.iii for Site Elements states that new ADA curb cuts on historic sidewalks should be installed to be consistent with the existing sidewalk color and texture while minimizing damage to the historic sidewalk. The 200 block of Greenlawn does not currently feature a historic sidewalk on the north or south sides, however it does feature curbing. The Greenlawn Estates Historic District currently features red truncated dome panels. Staff finds that the installation of new truncated dome panels should match existing truncated dome pads in the district.
- e. **ARCHAEOLOGY** – The project shall comply with all federal, state, and local laws, rules, and regulations regarding archaeology, as applicable.

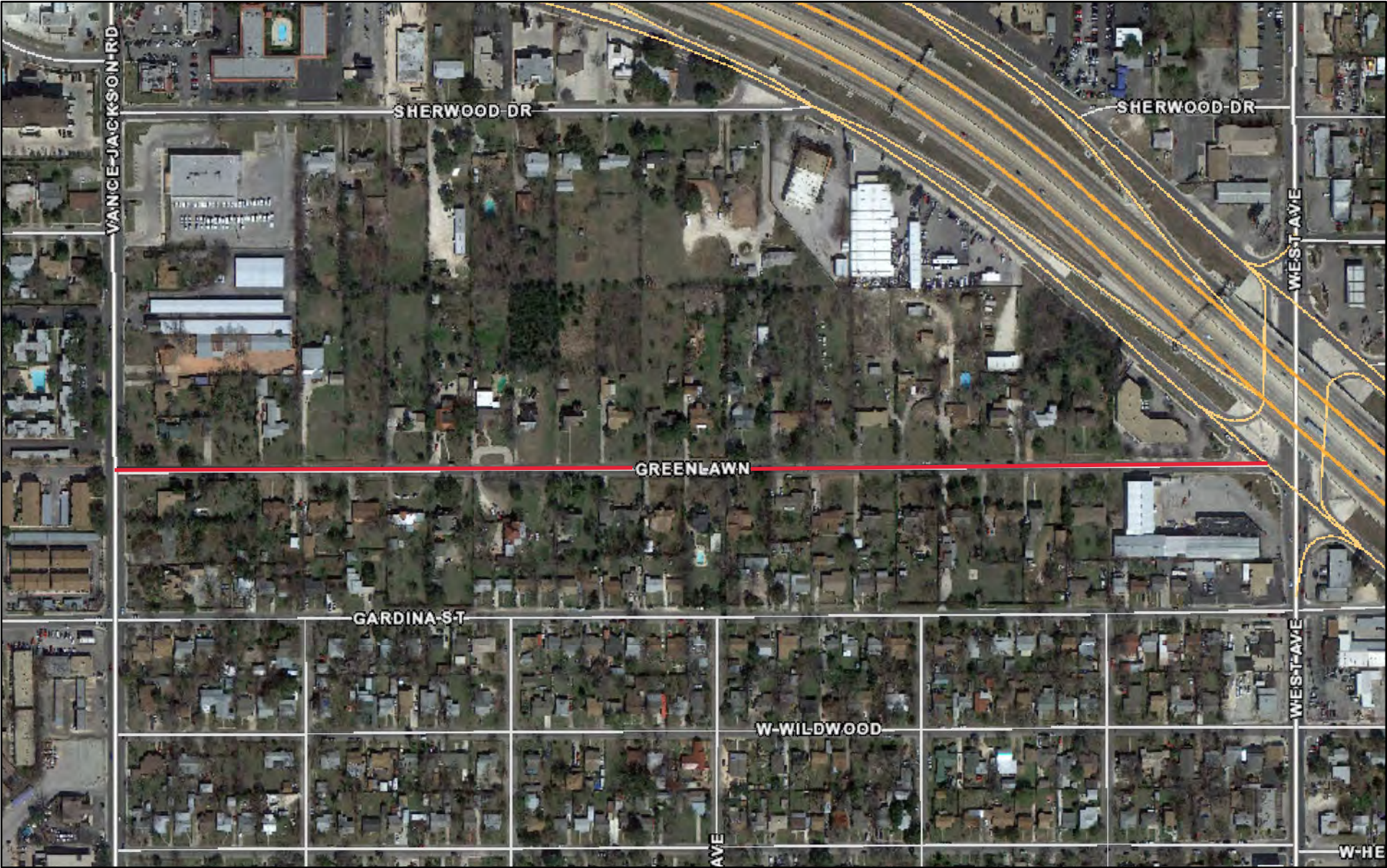
## **RECOMMENDATION:**

Staff recommends approval for the sidewalk installation on Greenlawn Dr with the following stipulations:

- i. That the sidewalk will be 4-feet wide and should match the existing dimensions already existing on the block; curbing should match the existing historic square profile
- ii. That the new proposed driveway approaches should not exceed the existing general width that already exists.
- iii. That the historic curbing is retained and any curbing that requires replacement matches the existing in material, width, and profile.
- iv. That the applicant maintains the width of existing driveway approaches and walkways. Any modifications to the footprint of the existing driveway approaches and walkways will require additional review and approval by staff.
  - i. That the applicant installs truncated dome panels to match the existing truncated dome panels in the district.
  - ii. **ARCHAEOLOGY** – The project shall comply with all federal, state, and local laws, rules, and regulations regarding archaeology, as applicable.

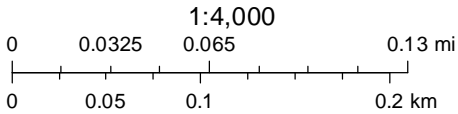


# City of San Antonio One Stop

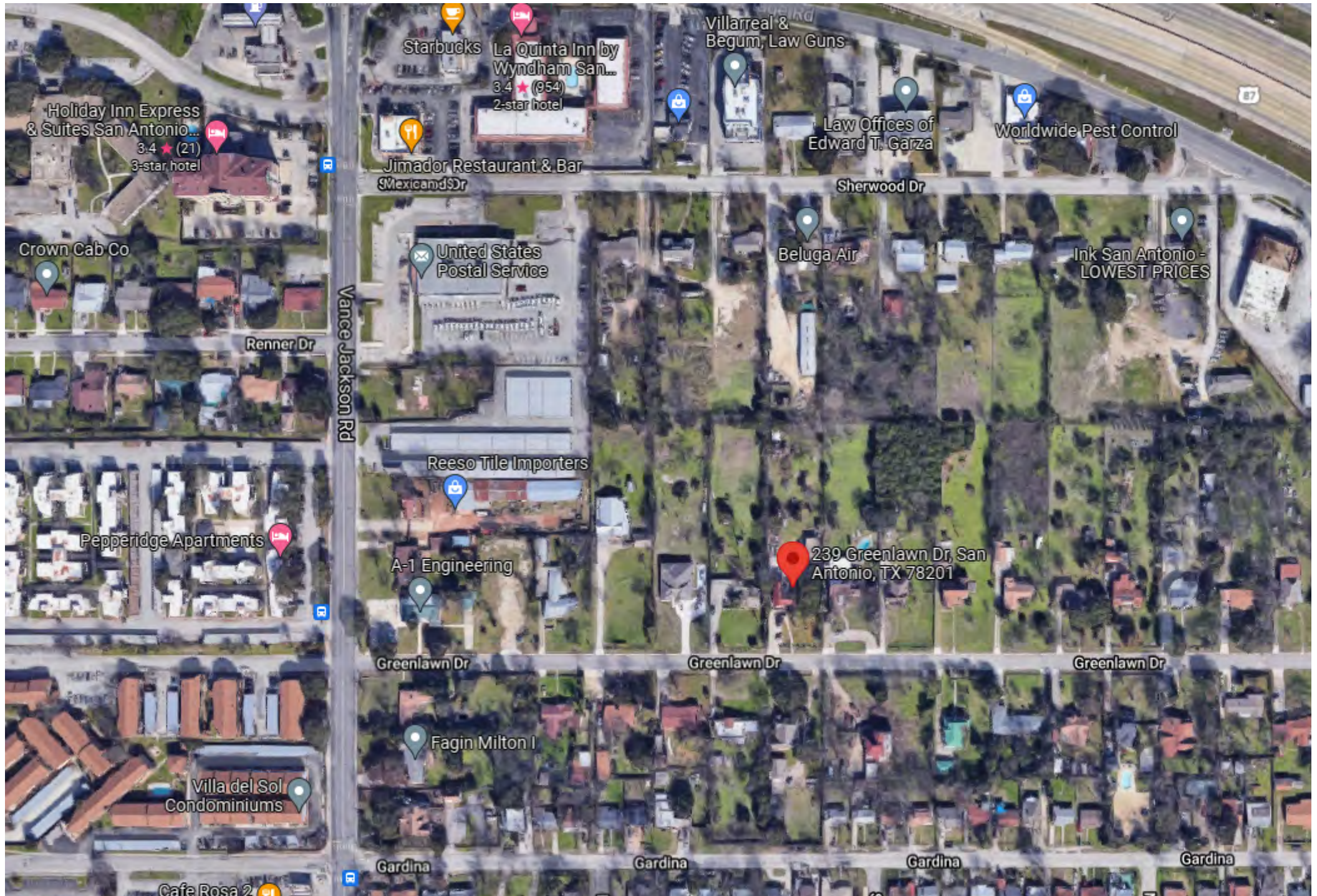


February 11, 2022

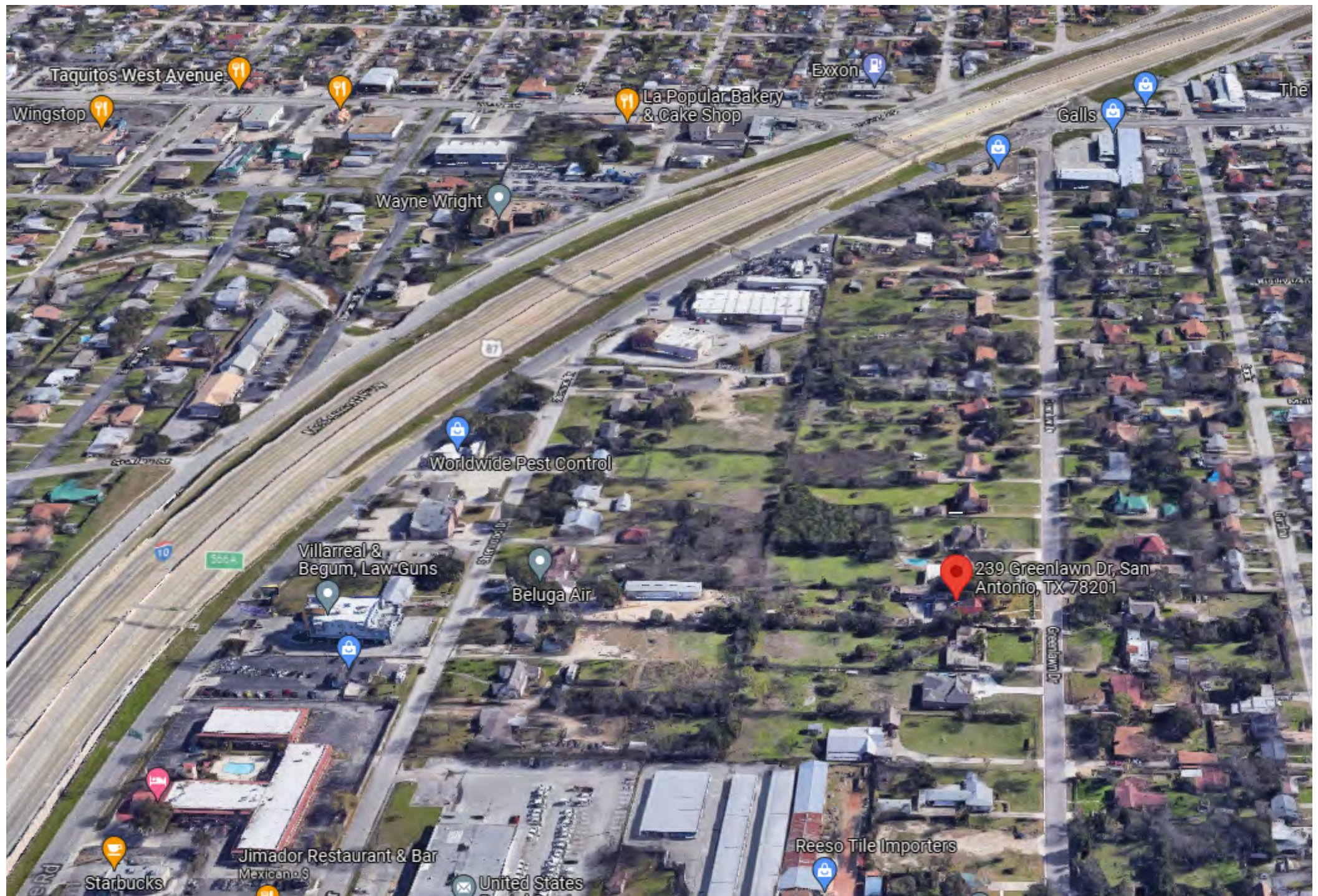
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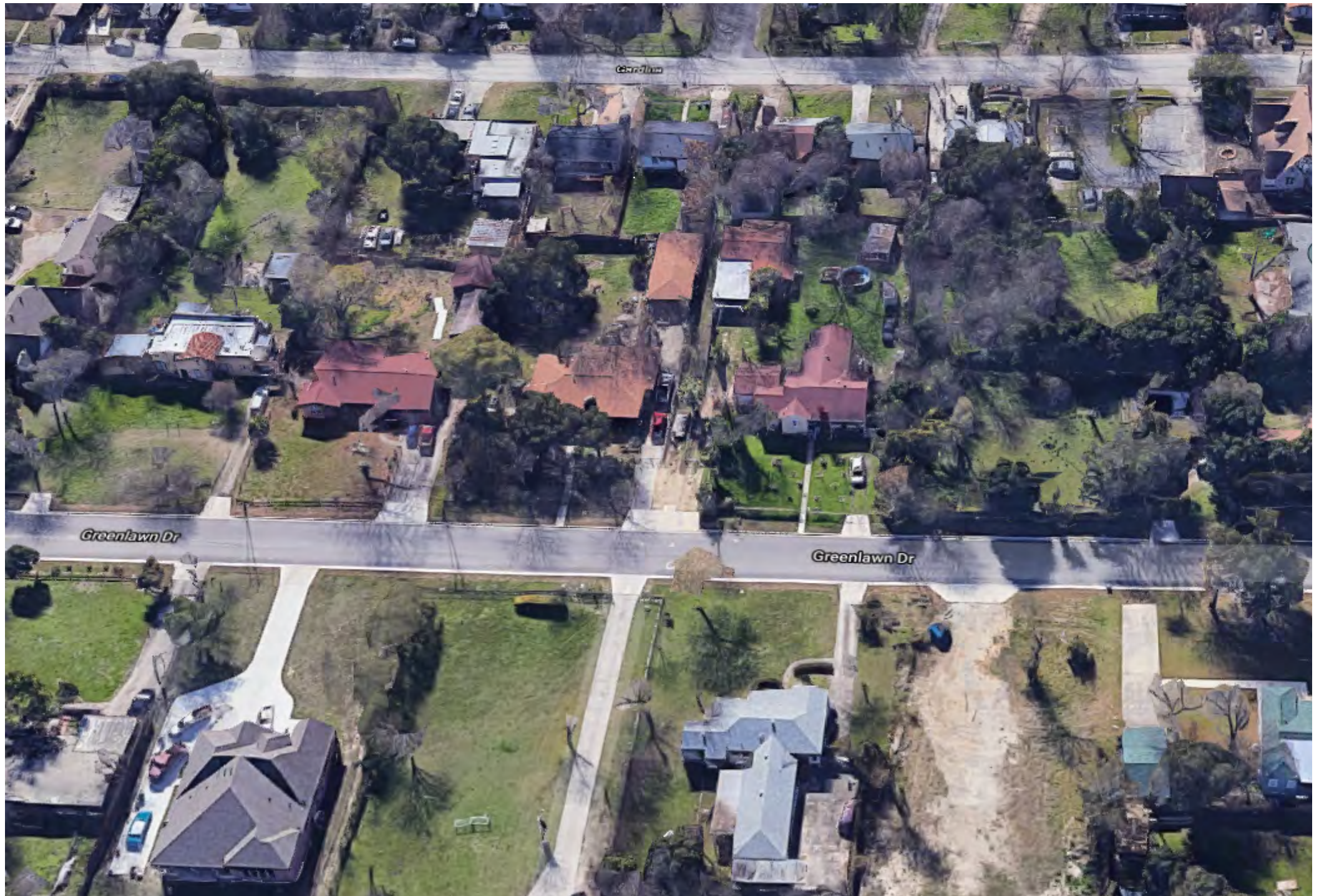




















# Greenlawn (Vance Jackson to Frontage)

## Sheet 2 of 5 Driveway Table

Driveway #	Address	Curb Width	Back Width	Depth	Type	Area (SF)
6	G.D. 249	18	14	10	R	160
7	G.D. 245	18	14	10	R	160
8	G.D. 239	24	20	10	R	220
9	G.D. 233	24	20	10	R	220
10	G.D. 233	24	20	10	R	220
11	G.D. 227	14	10	10	R	120

GENERAL NOTES  
 - ASPHALT CUT & PATCH SHALL BE 1" OR LESS  
 - SLOPE ASPHALT PATCH AWAY FROM RAMP/CURB  
 - INSPECTIONS SHALL CLEAR ALL SITEWORK & ISSUE  
 CERTIFICATE PRIOR TO PLACING CONCRETE  
 COORDINATE WITH INSPECTIONS AS REQUIRED

LEGEND	
	INSTALL
	EXISTING
	REMOVE
	CONCRETE
	ASPHALT
	PAVEMENT
	PAVEMENT

DATE	INIT.	DATE
DESIGNED BY: KAJ		
DRAWN BY: KAJ		
CHECKED BY: XJ11		
PROJECT No.		

CITY OF SAN ANTONIO  
 SIDEWALKS & SPECIAL PROJIS  
 - SIDEWALK CONSTRUCTION -