

HISTORIC AND DESIGN REVIEW COMMISSION

November 06, 2024

HDRC CASE NO: 2024-339
ADDRESS: 418 FLORIDA ST
319 E CAROLINA ST
LEGAL DESCRIPTION: NCB 3011 BLK 9 LOT 20
ZONING: C-2, H
CITY COUNCIL DIST.: 1
DISTRICT: Lavaca Historic District
APPLICANT: David Ranjbar/Prime & Paradigm Construction, LLC.
TYPE OF WORK: Construction of a commercial structure and fuel canopy; site work
APPLICATION RECEIVED: October 01, 2024
60-DAY REVIEW: November 30, 2024
CASE MANAGER: Edward Hall

REQUEST:

The applicant is requesting conceptual approval to construct a commercial structure with a detached fuel canopy on the vacant lot addressed as 418 Florida and 319 E Carolina, located within the Lavaca Historic District.

APPLICABLE CITATIONS:

Historic Design Guidelines, Chapter 4, Guidelines for New Construction

1. Building and Entrance Orientation

A. FAÇADE ORIENTATION

- i. Setbacks*—Align front facades of new buildings with front facades of adjacent buildings where a consistent setback has been established along the street frontage. Use the median setback of buildings along the street frontage where a variety of setbacks exist. Refer to UDC Article 3, Division 2. Base Zoning Districts for applicable setback requirements.
- ii. Orientation*—Orient the front façade of new buildings to be consistent with the predominant orientation of historic buildings along the street frontage.

B. ENTRANCES

- i. Orientation*—Orient primary building entrances, porches, and landings to be consistent with those historically found along the street frontage. Typically, historic building entrances are oriented towards the primary street.

2. Building Massing and Form

A. SCALE AND MASS

- i. Similar height and scale*—Design new construction so that its height and overall scale are consistent with nearby historic buildings. In residential districts, the height and scale of new construction should not exceed that of the majority of historic buildings by more than one-story. In commercial districts, building height shall conform to the established pattern. If there is no more than a 50% variation in the scale of buildings on the adjacent block faces, then the height of the new building shall not exceed the tallest building on the adjacent block face by more than 10%.
- ii. Transitions*—Utilize step-downs in building height, wall-plane offsets, and other variations in building massing to provide a visual transition when the height of new construction exceeds that of adjacent historic buildings by more than one-half story.
- iii. Foundation and floor heights*—Align foundation and floor-to-floor heights (including porches and balconies) within one foot of floor-to-floor heights on adjacent historic structures.

B. ROOF FORM

- i. Similar roof forms*—Incorporate roof forms—pitch, overhangs, and orientation—that are consistent with those predominantly found on the block. Roof forms on residential building types are typically sloped, while roof forms on nonresidential building types are more typically flat and screened by an ornamental parapet wall.

ii. Façade configuration—The primary façade of new commercial buildings should be in keeping with established patterns. Maintaining horizontal elements within adjacent cap, middle, and base precedents will establish a consistent street wall through the alignment of horizontal parts. Avoid blank walls, particularly on elevations visible from the street. No new façade should exceed 40 linear feet without being penetrated by windows, entryways, or other defined bays.

D. LOT COVERAGE

i. Building to lot ratio—New construction should be consistent with adjacent historic buildings in terms of the building to lot ratio. Limit the building footprint for new construction to no more than 50 percent of the total lot area, unless adjacent historic buildings establish a precedent with a greater building to lot ratio.

3. Materials and Textures

A. NEW MATERIALS

i. Complementary materials—Use materials that complement the type, color, and texture of materials traditionally found in the district. Materials should not be so dissimilar as to distract from the historic interpretation of the district. For example, corrugated metal siding would not be appropriate for a new structure in a district comprised of homes with wood siding.

ii. Alternative use of traditional materials—Consider using traditional materials, such as wood siding, in a new way to provide visual interest in new construction while still ensuring compatibility.

iii. Roof materials—Select roof materials that are similar in terms of form, color, and texture to traditionally used in the district.

iv. Metal roofs—Construct new metal roofs in a similar fashion as historic metal roofs. Refer to the Guidelines for Alterations and Maintenance section for additional specifications regarding metal roofs.

v. Imitation or synthetic materials—Do not use vinyl siding, plastic, or corrugated metal sheeting. Contemporary materials not traditionally used in the district, such as brick or simulated stone veneer and Hardie Board or other fiberboard siding, may be appropriate for new construction in some locations as long as new materials are visually similar to the traditional material in dimension, finish, and texture. EIFS is not recommended as a substitute for actual stucco.

4. Architectural Details

A. GENERAL

i. Historic context—Design new buildings to reflect their time while respecting the historic context. While new construction should not attempt to mirror or replicate historic features, new structures should not be so dissimilar as to distract from or diminish the historic interpretation of the district.

ii. Architectural details—Incorporate architectural details that are in keeping with the predominant architectural style along the block face or within the district when one exists. Details should be simple in design and should complement, but not visually compete with, the character of the adjacent historic structures or other historic structures within the district.

Architectural details that are more ornate or elaborate than those found within the district are inappropriate.

iii. Contemporary interpretations—Consider integrating contemporary interpretations of traditional designs and details for new construction. Use of contemporary window moldings and door surroundings, for example, can provide visual interest while helping to convey the fact that the structure is new. Modern materials should be implemented in a way that does not distract from the historic structure.

5. Garages and Outbuildings

A. DESIGN AND CHARACTER

i. Massing and form—Design new garages and outbuildings to be visually subordinate to the principal historic structure in terms of their height, massing, and form.

ii. Building size—New outbuildings should be no larger in plan than 40 percent of the principal historic structure footprint.

iii. Character—Relate new garages and outbuildings to the period of construction of the principal building on the lot through the use of complementary materials and simplified architectural details.

- iv. Windows and doors—Design window and door openings to be similar to those found on historic garages or outbuildings in the district or on the principle historic structure in terms of their spacing and proportions.
- v. Garage doors—Incorporate garage doors with similar proportions and materials as those traditionally found in the district.

B. SETBACKS AND ORIENTATION

- i. Orientation—Match the predominant garage orientation found along the block. Do not introduce front-loaded garages or garages attached to the primary structure on blocks where rear or alley loaded garages were historically used.
 - ii. Setbacks—Follow historic setback pattern of similar structures along the streetscape or district for new garages and outbuildings. Historic garages and outbuildings are most typically located at the rear of the lot, behind the principal building. In some instances, historic setbacks are not consistent with UDC requirements and a variance may be required.
- ### 6. Mechanical Equipment and Roof Appurtenances

A. LOCATION AND SITING

- i. *Visibility*—Do not locate utility boxes, air conditioners, rooftop mechanical equipment, skylights, satellite dishes, and other roof appurtenances on primary facades, front-facing roof slopes, in front yards, or in other locations that are clearly visible from the public right-of-way.
- ii. *Service Areas*—Locate service areas towards the rear of the site to minimize visibility from the public right-of-way.

B. SCREENING

- i. *Building-mounted equipment*—Paint devices mounted on secondary facades and other exposed hardware, frames, and piping to match the color scheme of the primary structure or screen them with landscaping.
 - ii. *Freestanding equipment*—Screen service areas, air conditioning units, and other mechanical equipment from public view using a fence, hedge, or other enclosure.
 - iii. *Roof-mounted equipment*—Screen and set back devices mounted on the roof to avoid view from public right-of-way.
- Historic Design Guidelines, Chapter 5, Guidelines for Site Elements

Historic Design Guidelines, Chapter 5, Guidelines for Site Elements

B. NEW FENCES AND WALLS

- i. *Design*—New fences and walls should appear similar to those used historically within the district in terms of their scale, transparency, and character. Design of fence should respond to the design and materials of the house or main structure.
- ii. *Location*—Avoid installing a fence or wall in a location where one did not historically exist, particularly within the front yard. The appropriateness of a front yard fence or wall is dependent on conditions within a specific historic district. New front yard fences or wall should not be introduced within historic districts that have not historically had them.
- iii. *Height*—Limit the height of new fences and walls within the front yard to a maximum of four feet. The appropriateness of a front yard fence is dependent on conditions within a specific historic district. New front yard fences should not be introduced within historic districts that have not historically had them. If a taller fence or wall existed historically, additional height may be considered. The height of a new retaining wall should not exceed the height of the slope it retains.
- iv. *Prohibited materials*—Do not use exposed concrete masonry units (CMU), Keystone or similar interlocking retaining wall systems, concrete block, vinyl fencing, or chain link fencing.
- v. *Appropriate materials*—Construct new fences or walls of materials similar to fence materials historically used in the district. Select materials that are similar in scale, texture, color, and form as those historically used in the district, and that are compatible with the main structure. Screening incompatible uses—Review alternative fence heights and materials for appropriateness where residential properties are adjacent to commercial or other potentially incompatible uses.

3. Landscape Design

A. PLANTINGS

- i. *Historic Gardens*—Maintain front yard gardens when appropriate within a specific historic district.
- ii. *Historic Lawns*—Do not fully remove and replace traditional lawn areas with impervious hardscape. Limit the removal of lawn areas to mulched planting beds or pervious hardscapes in locations where they would historically be

- found, such as along fences, walkways, or drives. Low-growing plantings should be used in historic lawn areas; invasive or large-scale species should be avoided. Historic lawn areas should never be reduced by more than 50%.
- iii. *Native xeric plant materials*—Select native and/or xeric plants that thrive in local conditions and reduce watering usage. See UDC Appendix E: San Antonio Recommended Plant List—All Suited to Xeriscape Planting Methods, for a list of appropriate materials and planting methods. Select plant materials with a similar character, growth habit, and light requirements as those being replaced.
 - iv. *Plant palettes*—If a varied plant palette is used, incorporate species of taller heights, such informal elements should be restrained to small areas of the front yard or to the rear or side yard so as not to obstruct views of or otherwise distract from the historic structure.
 - v. *Maintenance*—Maintain existing landscape features. Do not introduce landscape elements that will obscure the historic structure or are located as to retain moisture on walls or foundations (e.g., dense foundation plantings or vines) or as to cause damage.

B. ROCKS OR HARDSCAPE

- i. *Impervious surfaces* —Do not introduce large pavers, asphalt, or other impervious surfaces where they were not historically located.
- ii. *Pervious and semi-pervious surfaces*—New pervious hardscapes should be limited to areas that are not highly visible, and should not be used as wholesale replacement for plantings. If used, small plantings should be incorporated into the design.
- iii. *Rock mulch and gravel* - Do not use rock mulch or gravel as a wholesale replacement for lawn area. If used, plantings should be incorporated into the design.

D. TREES

- i. *Preservation*—Preserve and protect from damage existing mature trees and heritage trees. See UDC Section 35-523 (Tree Preservation) for specific requirements.
- ii. *New Trees* – Select new trees based on site conditions. Avoid planting new trees in locations that could potentially cause damage to a historic structure or other historic elements. Species selection and planting procedure should be done in accordance with guidance from the City Arborist.

5. Sidewalks, Walkways, Driveways, and Curbing

A. SIDEWALKS AND WALKWAYS

- i. *Maintenance*—Repair minor cracking, settling, or jamming along sidewalks to prevent uneven surfaces. Retain and repair historic sidewalk and walkway paving materials—often brick or concrete—in place.
- ii. *Replacement materials*—Replace those portions of sidewalks or walkways that are deteriorated beyond repair. Every effort should be made to match existing sidewalk color and material.
- iii. *Width and alignment*—Follow the historic alignment, configuration, and width of sidewalks and walkways. Alter the historic width or alignment only where absolutely necessary to accommodate the preservation of a significant tree.
- iv. *Stamped concrete*—Preserve stamped street names, business insignias, or other historic elements of sidewalks and walkways when replacement is necessary.
- v. *ADA compliance*—Limit removal of historic sidewalk materials to the immediate intersection when ramps are added to address ADA requirements.

B. DRIVEWAYS

- i. *Driveway configuration*—Retain and repair in place historic driveway configurations, such as ribbon drives. Incorporate a similar driveway configuration—materials, width, and design—to that historically found on the site. Historic driveways are typically no wider than 10 feet. Pervious paving surfaces may be considered where replacement is necessary to increase stormwater infiltration.
- ii. *Curb cuts and ramps*—Maintain the width and configuration of original curb cuts when replacing historic driveways. Avoid introducing new curb cuts where not historically found.

7. Off-Street Parking

A. LOCATION

- i. Preferred location*—Place parking areas for non-residential and mixed-use structures at the rear of the site, behind primary structures to hide them from the public right-of-way. On corner lots, place parking areas behind the primary structure and set them back as far as possible from the side streets. Parking areas to the side of the primary structure are acceptable when location behind the structure is not feasible. See UDC Section 35-310 for district-specific standards.
- ii. Front*—Do not add off-street parking areas within the front yard setback as to not disrupt the continuity of the streetscape.
- iii. Access*—Design off-street parking areas to be accessed from alleys or secondary streets rather than from principal streets whenever possible.

B. DESIGN

- i. Screening*—Screen off-street parking areas with a landscape buffer, wall, or ornamental fence two to four feet high—or a combination of these methods. Landscape buffers are preferred due to their ability to absorb carbon dioxide. See UDC Section 35-510 for buffer requirements.
- ii. Materials*—Use permeable parking surfaces when possible to reduce run-off and flooding. See UDC Section 35-526(j) for specific standards.
- iii. Parking structures*—Design new parking structures to be similar in scale, materials, and rhythm of the surrounding historic district when new parking structures are necessary.

Standard Specifications for Windows in Additions and New Construction

Consistent with the Historic Design Guidelines, the following recommendations are made for windows to be used in new construction:

- **GENERAL:** Windows used in new construction should be similar in appearance to those commonly found within the district in terms of size, profile, and configuration. While no material is expressly prohibited by the Historic Design Guidelines, a high quality wood or aluminum-clad wood window product often meets the Guidelines with the stipulations listed below.
- **SIZE:** Windows should feature traditional dimensions and proportions as found within the district.
- **SASH:** Meeting rails must be no taller than 1.25". Stiles must be no wider than 2.25". Top and bottom sashes must be equal in size unless otherwise approved.
- **DEPTH:** There should be a minimum of 2" in depth between the front face of the window trim and the front face of the top window sash. This must be accomplished by recessing the window sufficiently within the opening or with the installation of additional window trim to add thickness. All windows should be supplied in a block frame and exclude nailing fins which limit the ability to sufficiently recess the windows.
- **TRIM:** Window trim must feature traditional dimensions and architecturally appropriate casing and sloped sill detail.
- **GLAZING:** Windows should feature clear glass. Low-e or reflective coatings are not recommended for replacements. The glazing should not feature faux divided lights with an interior grille. If approved to match a historic window configuration, the window should feature true, exterior muntins.
- **COLOR:** Wood windows should feature a painted finish. If a clad or non-wood product is approved, white or metallic manufacturer's color is not allowed and color selection must be presented to staff.

FINDINGS:

- a. The applicant is requesting a Certificate of Appropriateness for approval to construct a commercial structure with a detached fuel canopy on the vacant lot addressed as 418 Florida and 319 E Carolina, located within the Lavaca Historic District.
- b. **DESIGN REVIEW COMMITTEE** – This request was reviewed by the Design Review Committee on October 8, 2024. At that meeting, committee members commented on building setbacks, site configuration, architectural details, signage and lighting. This request was reviewed a second time by the Design Review Committee on October 29, 2024. At that meeting, committee members comments on the building's form and architecture, and made recommendations regarding façade arrangement, fenestration, and architectural details.
- c. **CONTEXT & DEVELOPMENT PATTERN** – As noted in finding a, this lot is vacant and void of structures. The lot is bounded by Florida Street to the north, E Carolina Street to the south, the IH-27 Access Road to the east, and single-family residential structure to the west. Across both Florida and E Carolina are single-family residential structures. Both blocks of Florida and E Carolina are composed of historic, residential structures.

- d. **SETBACKS & ORIENTATION** – According to the Guidelines for New Construction, the front facades of new buildings are to align with front facades of adjacent buildings where a consistent setback has been established along the street frontage. The applicant has proposed to orient the proposed new construction to the east, towards IH-37. The applicant has noted that the proposed new construction will feature a setback on Florida Street that is greater than the front setbacks of the adjacent, historic structures. Staff finds the proposed setback from Florida Street to be appropriate; however, staff finds that architectural elements should be added to the north (Florida) façade to address Florida Street. The historic development pattern on Florida Street features historic structures that all feature architectural orientations towards Florida Street. Staff finds that the proposed new construction should feature architectural elements on its north façade that address Florida Street, such as an entrance canopy, facade separation, complimentary materials, and fenestration detail.
- e. **ENTRANCES** – Per the application drawings, three, front facing entrances have been proposed. These entrances are located within storefront systems and beneath an entrance canopy that spans the front façade. Staff finds the proposed entrances and their orientation to be appropriate and consistent with the Guidelines.
- f. **SCALE & MASS** – Per the Guidelines for New Construction 2.A.i., a height and massing similar to historic structures in the vicinity of the proposed new construction should be used. In residential districts, the height and scale of new construction should not exceed that of the majority of historic buildings by more than one-story. The applicant has proposed for the new construction to feature one story in height; approximately twenty-four (24) feet. Generally, staff finds the proposed height to be appropriate; however, staff finds that a street elevation should be developed to show the proposed height of twenty-four (24) feet in context with the adjacent, single-story historic structures on E Carolina and Florida Streets.
- g. **FOUNDATION & FLOOR HEIGHTS** – According to the Guidelines for New Construction 2.a.iii., foundation and floor heights should be aligned within one (1) foot of neighboring structure's foundation and floor heights. Generally, the applicant has proposed foundation and floor heights that are consistent with historic, commercial structures found within the Lavaca Historic District.
- h. **ROOF FORM** – The Guidelines for New Construction 2.B.i. note that roof forms that are consistent with those predominantly found on the block should be incorporated into new construction. Historic commercial structures found within the Lavaca Historic District feature flat roofs with parapet walls, which the applicant has proposed to incorporate within the new construction. Staff finds the proposed roof form to be appropriate.
- i. **LOT COVERAGE** – The Guidelines for New Construction 2.D.i. notes that new construction should be consistent with adjacent historic buildings in terms of the building to lot ratio. Footprints of new construction should be limited to no more than fifty (50) percent of the total lot area, unless adjacent historic buildings establish a precedent with a greater building to lot ratio. Per BCAD, the total lot size is .9298 acres, or 40,500 square feet. The applicant has proposed for the commercial structure to feature a footprint of 7,200 square feet and the fuel canopy to feature a footprint of 3,936 square feet. The proposed lot coverage is consistent with the Guidelines for New Construction.
- j. **MATERIALS** – The applicant has proposed materials that include stucco and brick. Along with these, the applicant has proposed metal façade canopies and storefront systems. Generally, staff finds the proposed materials to be appropriate and consistent with the Guidelines; however, staff finds that stucco should feature a traditional finish and profile. Brick should be true, dimensional brick, and storefront systems and canopies should be profiled and detailed to be consistent with those found historically within the Lavaca Historic District. Specifications of each elements should be submitted for review and approval. Materials and their profiles should reference those of historic, commercial structures in the Lavaca Historic District. Commercial, suburban elements and details should not be incorporated into the design.
- k. **FENESTRATION PROFILE** – The applicant has proposed fenestration that consists of a storefront system with transom windows. Generally, staff finds the proposed approach to fenestration to be reflective of fenestration found on historically on commercial structures within the Lavaca Historic District. Staff finds that masonry knee walls should be added at each storefront opening. Additionally, staff finds that the profiles of muntins, mullions and other storefront and transom frame elements should relate to those found historically on commercial structures within the Lavaca Historic District.
- l. **ARCHITECTURAL DETAILS** – Generally, staff finds that the proposed new construction features architectural elements that relate to those found on historic commercial structures within the Lavaca Historic District. As noted in the above findings, staff finds that additional design elements should be added to the north façade, that brick knee walls should be added beneath each storefront system, that all storefront and transom systems should feature profiles that relate to those found historically within the district, and that materials, such as brick and stucco relate to that found historically within the district. When returning to the Commission for

final approval, the applicant should have fully developed construction documents and material details and specifications.

- m. LANDSCAPING – The applicant has noted the general location of landscaping elements on the site plan. Staff finds that a detailed landscaping plan should be developed and submitted for review and approval when returning to the Commission for final approval. All landscaping requirements of the Unified Development Code and the Historic Design Guidelines are to be met. This includes a minimum 15-foot buffer yard along the west property line between the building and the residential properties. Staff finds that the building setback should be increased to 15 feet and that tall, evergreen shrubs or trees that screen the commercial use from residential properties should be incorporated into the landscape plan.
- n. FUEL CANOPY – The applicant has noted the installation of a fuel canopy to cover six (6) fuel pumps. The applicant has provided a site plan and renderings noting the location and general form and design of the fuel canopy. Staff finds that the canopy should feature simplified architectural elements with down lighting. The canopy should not feature internally illuminated signage, internally illuminated fascias, or excessive, commercial lighting. A complete construction document set for the proposed fuel canopy is to be submitted for review and approval when returning to the Commission for final approval.
- o. SITE & ARCHITECTURAL LIGHTING – The applicant has not submitted a site lighting plan at this time; however, there are a number of elements on site that will require lighting, including the proposed fuel canopy. Staff finds that a detailed site and architectural lighting plan should be developed and submitted for review and approval when returning to the Commission for final approval. Lighting should be developed in a manner that does not result in light pollution.
- p. SIGNAGE – The applicant has noted signage on the front façade of the proposed commercial structure, as well as signage on the fuel canopy, and two, monument signs. Staff finds that a master signage plan should be submitted for review and approval prior to the installation of signage. Additionally, staff finds that all signage should be indirectly lit and that only one monument sign should be installed on site. Pole or pylon signs are prohibited by code and should not be requested.
- q. MECHANICAL & SERVICE EQUIPMENT – All mechanical equipment should be installed in a manner where it is screened from view from the right of way. Rooftop mechanical equipment should be entirely screened by the proposed parapet walls. The applicant is responsible for appropriately and adequately screening all mechanical equipment.
- r. DUMPSTER LOCATION – The applicant has proposed to install a dumpster in the southeast corner of the lot. The dumpster has been proposed to be located within a concrete masonry unit wall. Generally, staff is concerned regarding this location as it would be located directly in front of the historic structure at 318 E Carolina. Staff finds that the applicant should consider the relocation or repositioning of the dumpster, and that the applicant should apply stucco to the concrete masonry unit screening walls.
- s. VEHICULAR ACCESS – The applicant has proposed two vehicular access points into the site; one on E Carolina and one on Florida, which will feature individual entrance and exit lanes. The applicant has noted a curbcut width of twenty-five (25) feet on E Carolina; however, the applicant has not noted a total width of curb cuts on Florida. Generally, staff finds the proposed curb cuts to be appropriate; however, staff finds that no curb cut should exceed twenty-five (25) feet in width total.
- t. ARCHAEOLOGY – The project area is partially located within the Lavaca Local Historic District. In addition, historical archival maps identify the Acequia del Alamo likely traversing the property. Therefore, an archaeological investigation is required. The project shall comply with all federal, state, and local laws, rules, and regulations regarding archaeology, as applicable.

RECOMMENDATION:

Staff does not recommend conceptual approval at this time due to conflicts with the UDC and Historic Design Guidelines. Staff recommends the applicant address the following items prior to receiving a recommendation for conceptual approval:

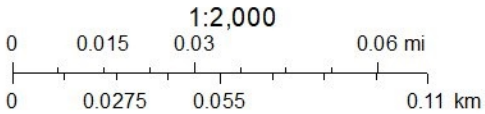
- i. That the proposed new construction should address architectural elements on its north façade that address Florida Street, such as an entrance canopy, brick and fenestration, as noted in finding d.
- ii. That a street elevation be developed to show the proposed height of twenty-four (24) feet in context with the adjacent, single-story historic structures on E Carolina and Florida Streets, as noted in finding f.
- iii. That brick be true, dimensional brick, and that stucco be applied in a traditional manner and feature a traditional finish and appearance, as noted in finding j.

- iv. That materials and their profiles should reference those of historic, commercial structures in the Lavaca Historic District. Commercial, suburban elements and details should not be incorporated into the design.
- v. That masonry knee walls be added at each storefront opening. Additionally, staff recommends that the profiles of muntins, mullions and other storefront and transom frame elements relate to those found historically on commercial structures within the Lavaca Historic District.
- vi. That a detailed master signage plan be proposed that features only one (1) monument sign, that all signage feature indirect lighting, and that no pole or pylon signs are proposed, as noted in finding o.
- vii. That all mechanical and service equipment be located in a manner than screens it from view from the public right of way, as noted in finding p.
- viii. That the applicant consider the relocation or repositioning of the dumpster, and that the applicant apply stucco to the concrete masonry unit screening walls, as noted in finding q.
- ix. That no curb cut exceed twenty-five (25) feet in width.
- x. That all setbacks as required by Zoning within the Unified Development Code are to be met. This includes a minimum 15-foot buffer yard along the west property line between the building and the residential properties.
- xi. That a detailed landscaping plan be developed and submitted for review and approval when returning to the Commission for final approval. Tall, evergreen shrubs or trees that screen the commercial use from residential properties should be incorporated into the landscape plan.
- xii. That a detailed site and architectural lighting plan be developed and submitted for review and approval when returning to the Commission for final approval. Lighting should be developed in a manner that does not result in light pollution.
- xiii. ARCHAEOLOGY – An archaeological investigation is required. The project shall comply with all federal, state, and local laws, rules, and regulations regarding archaeology, as applicable.

City of San Antonio One Stop



October 31, 2024





CITY OF SAN ANTONIO
**OFFICE OF HISTORIC
PRESERVATION**

Historic and Design Review Commission
Design Review Committee Report

DATE: October 8, 2024

HDRC Case #:

Address: 419 Florida

Meeting Location: Webex

APPLICANT: David Ranjbar

DRC Members present: Jeff Fetzer, Monica Savino

Staff Present: Edward Hall, Bryan Morales

Others present: Rahil

REQUEST:

COMMENTS/CONCERNS:

MS: Questions regarding previous fuel canopy on site.

JF: Questions regarding operating hours. Rahil: closes at 11pm M-T, Midnight F, S.

JF: Questions regarding drainage. Will water drain underground from downspouts and scuppers, how will water be prevented from draining towards adjacent properties. DR: Water will be directed to the landscaped areas.

JF: Water drainage should not impact neighbors.

JF: The horizontal banding creates a parapet on the east side of the building; please show rooftop equipment in elevations and to show how the equipment will be screened.

Discussion regarding RTU's and their screening.

MS: To meet side setback, why not remove five feet from the building footprint?

MS: Has brick been considered to relate to historic structures within the historic district? DR: Design is being revised to include brick and reference historic examples.

JF: Questions regarding location of dumpster enclosure.

JF: Provide adjacent historic structures (across the street on Carolina and Florida) and adjacent on west property lines to provide additional context.

JF: Has a traffic study been conducted? DR: In process.

MS: Questions regarding tree requirements. RJ: Will follow required planting. All existing trees will remain; existing trees are within the right of way and will remain.

OVERALL COMMENTS:



CITY OF SAN ANTONIO
**OFFICE OF HISTORIC
PRESERVATION**

Historic and Design Review Commission
Design Review Committee Report

DATE: October 29, 2024

HDRC Case #: 2024-339

Address: 418 Florida

Meeting Location: Webex

APPLICANT: David Ranjbar

DRC Members present: Jeff Fetzer, Monica Savino, Karen Burgard

Staff Present: Edward Hall, Cory Edwards

Others present:

REQUEST:

Construction of a commercial structure, fuel canopy, site and landscaping work

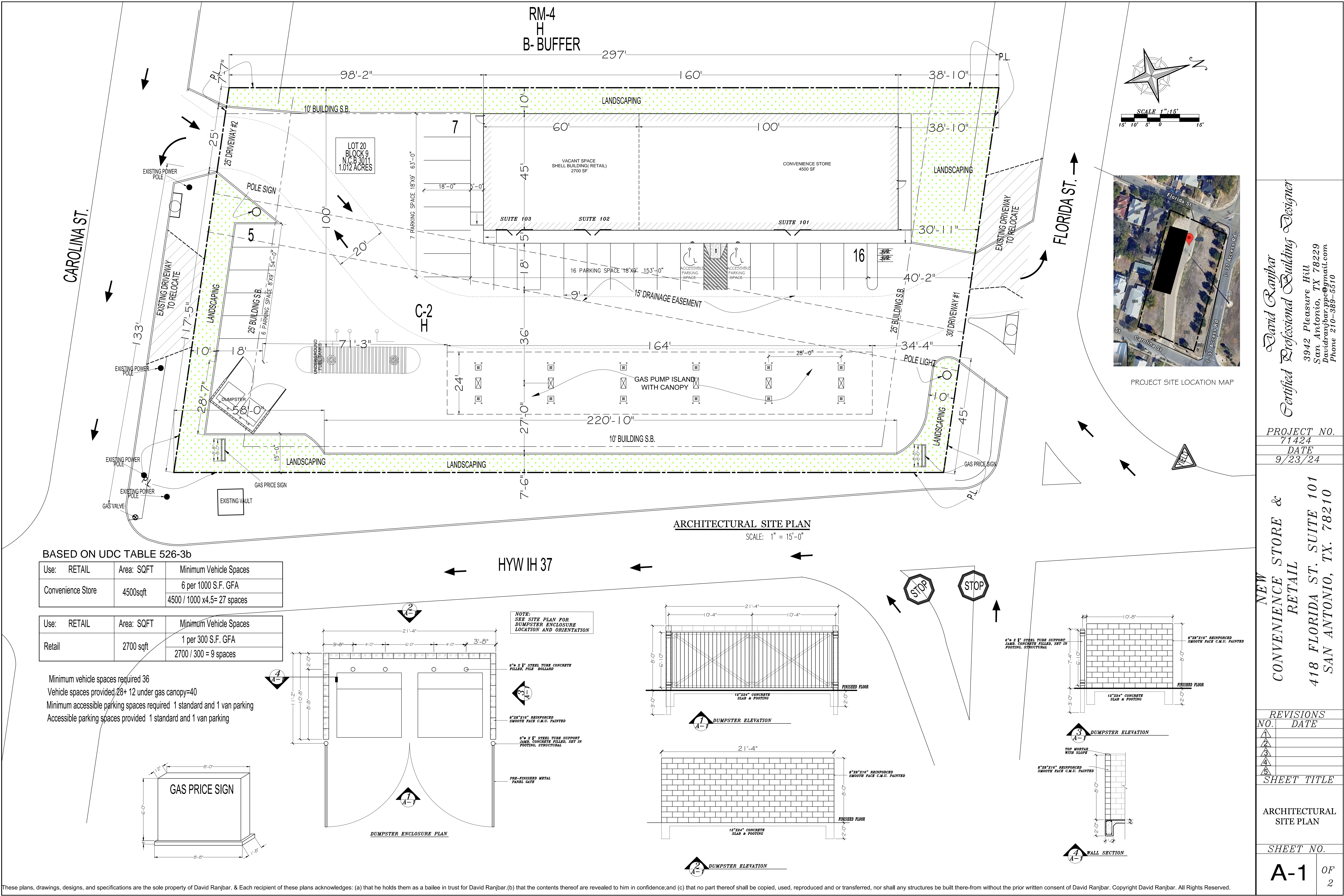
COMMENTS/CONCERNS:

MS: The updated design has improved significantly from the previous review. Questions about how the building relates to historic buildings in Lavaca.

MS: The design needs additional detailing.

JF: Consider how the north elevation can be improved. It does not relate to the existing context of the block. Consider an upgrade in design elements.

OVERALL COMMENTS:



BASED ON UDC TABLE 526-3b

Use:	RETAIL	Area: SQFT	Minimum Vehicle Spaces
Convenience Store	4500sqft		6 per 1000 S.F. GFA
			4500 / 1000 x4.5= 27 spaces

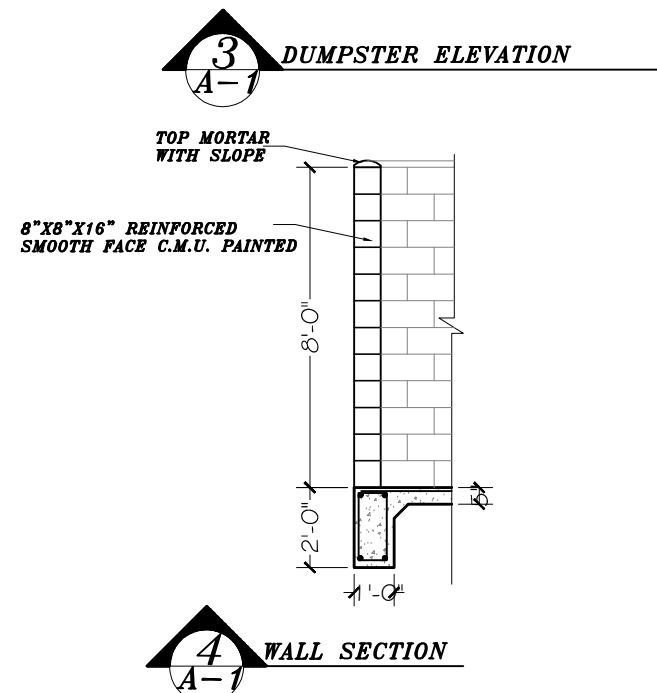
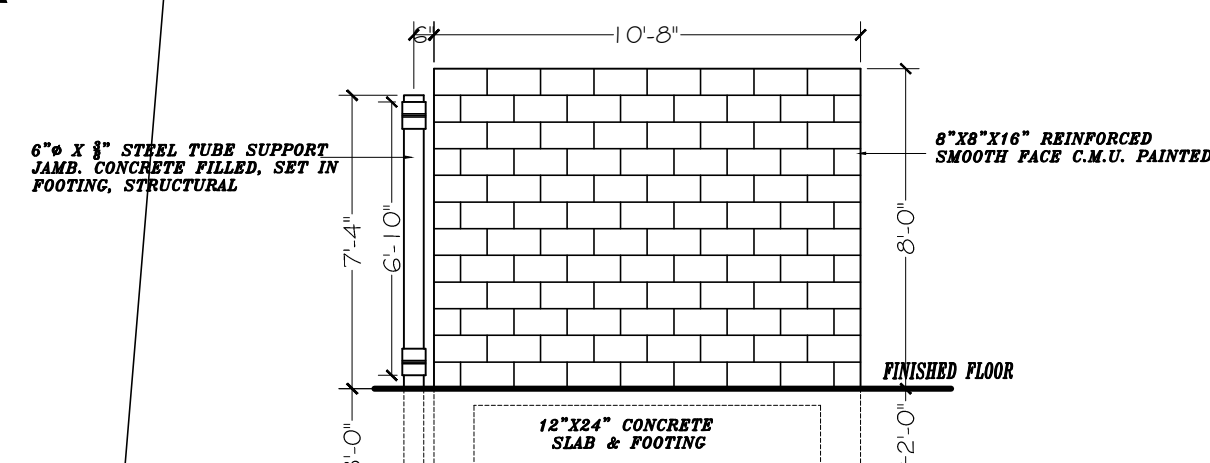
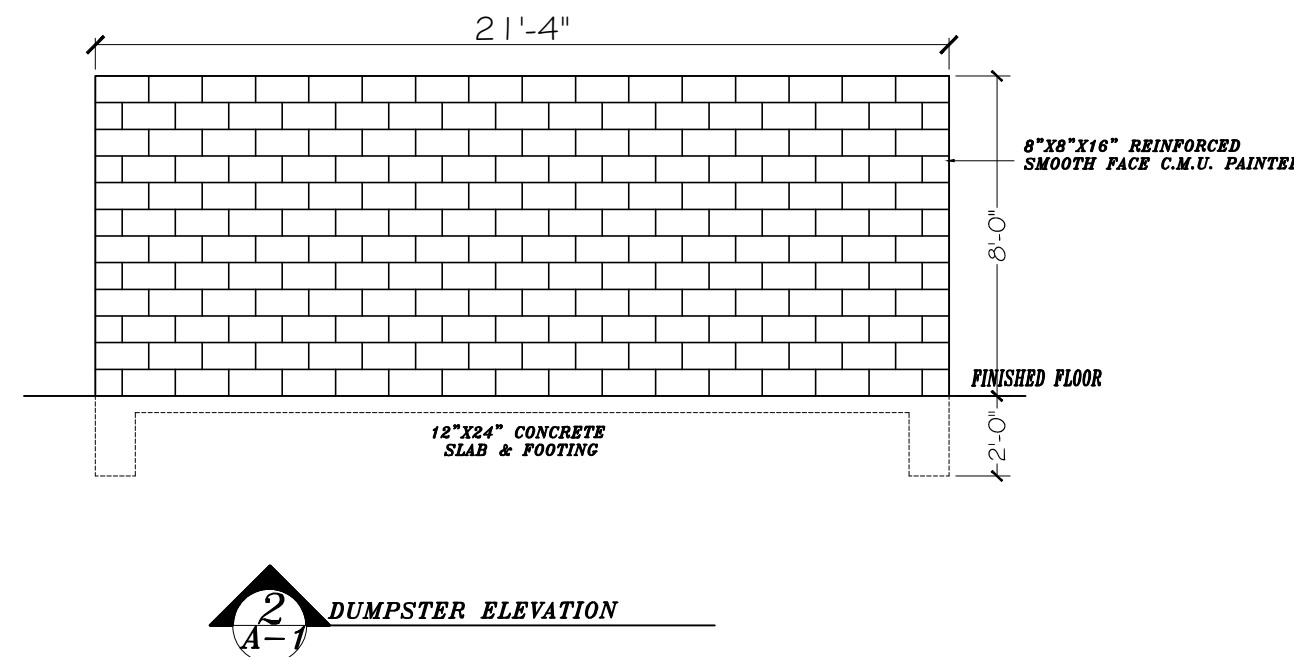
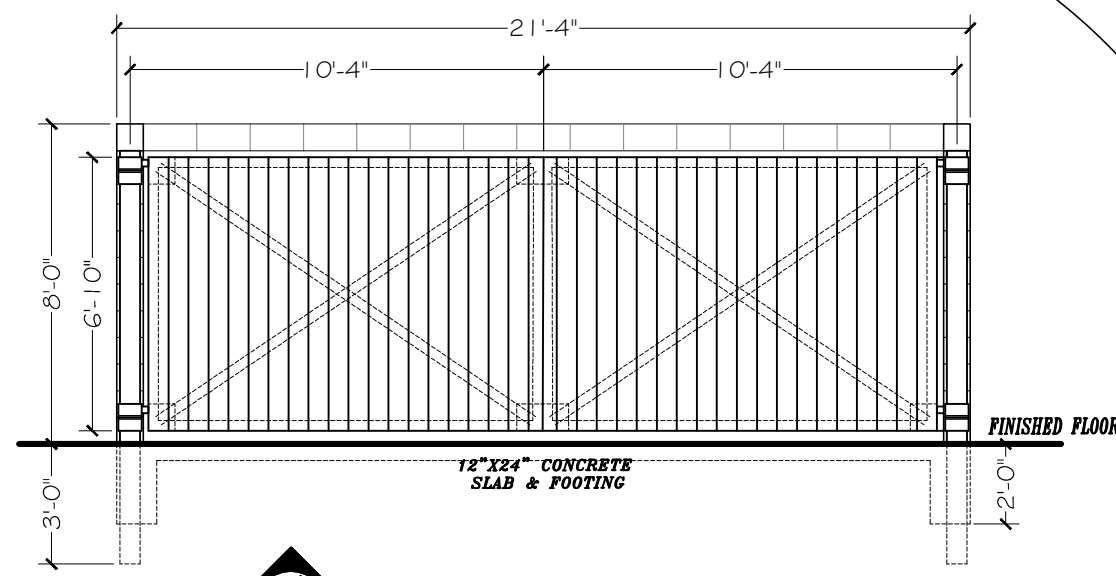
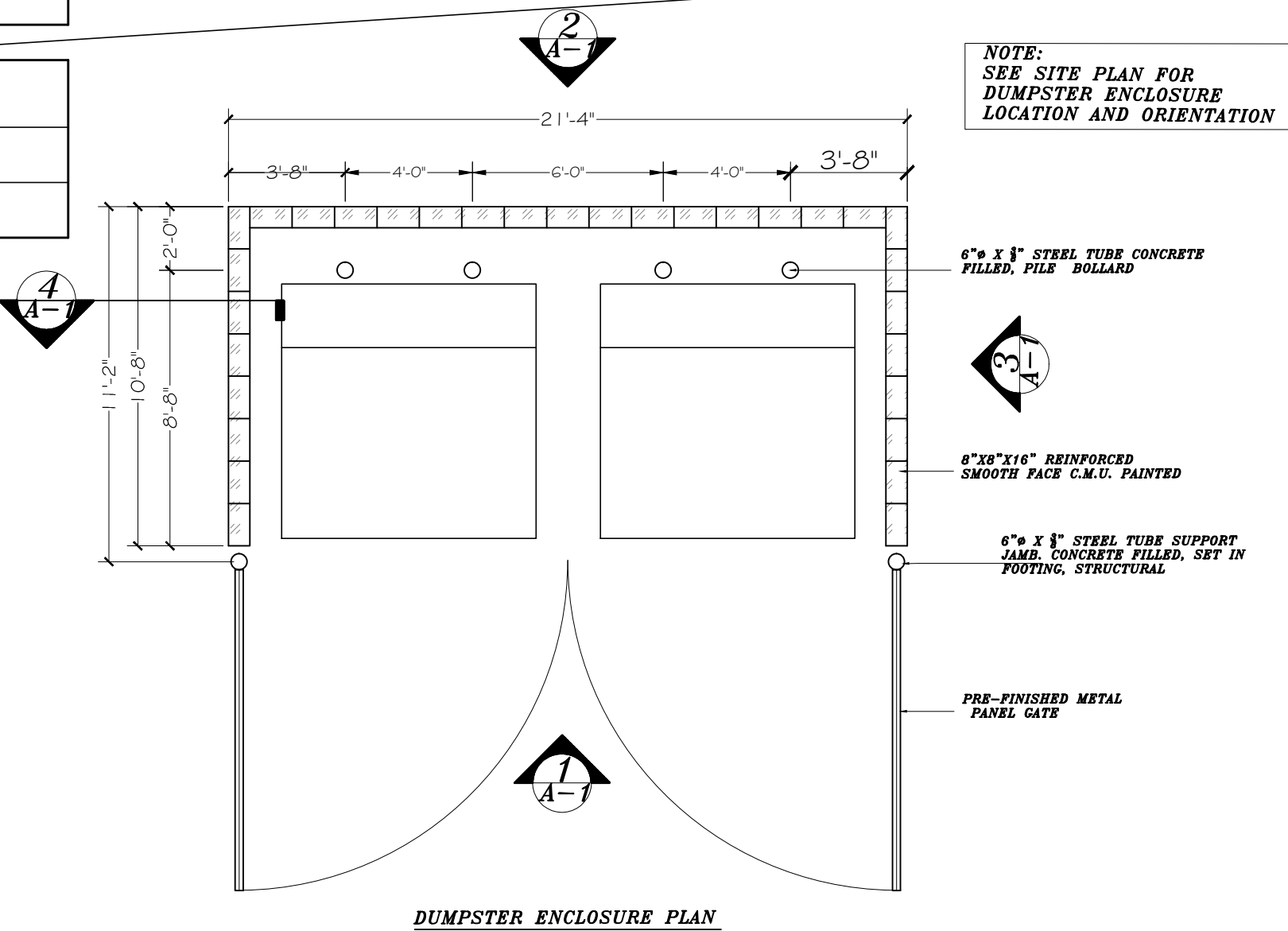
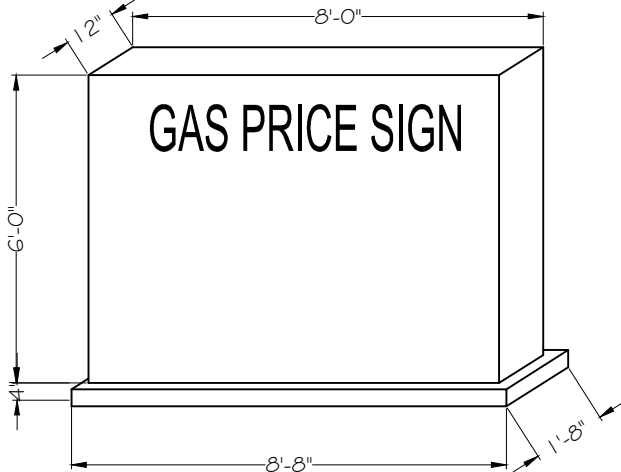
Use:	RETAIL	Area: SQFT	Minimum Vehicle Spaces
Retail	2700 sqft		1 per 300 S.F. GFA
			2700 / 300 = 9 spaces

Minimum vehicle spaces required 36

Vehicle spaces provided 28+ 12 under gas canopy=40

Minimum accessible parking spaces required 1 standard and 1 van parking

Accessible parking spaces provided 1 standard and 1 van parking



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Phone 210-389-5510

PROJECT NO.
71424
DATE
9/23/24

NEW
CONVENIENCE STORE &
RETAIL
418 FLORIDA ST. SUITE 101
SAN ANTONIO, TX. 78210

REVISIONS
NO. DATE

SHEET TITLE

ARCHITECTURAL
SITE PLAN

SHEET NO.

A-1 OF 2





















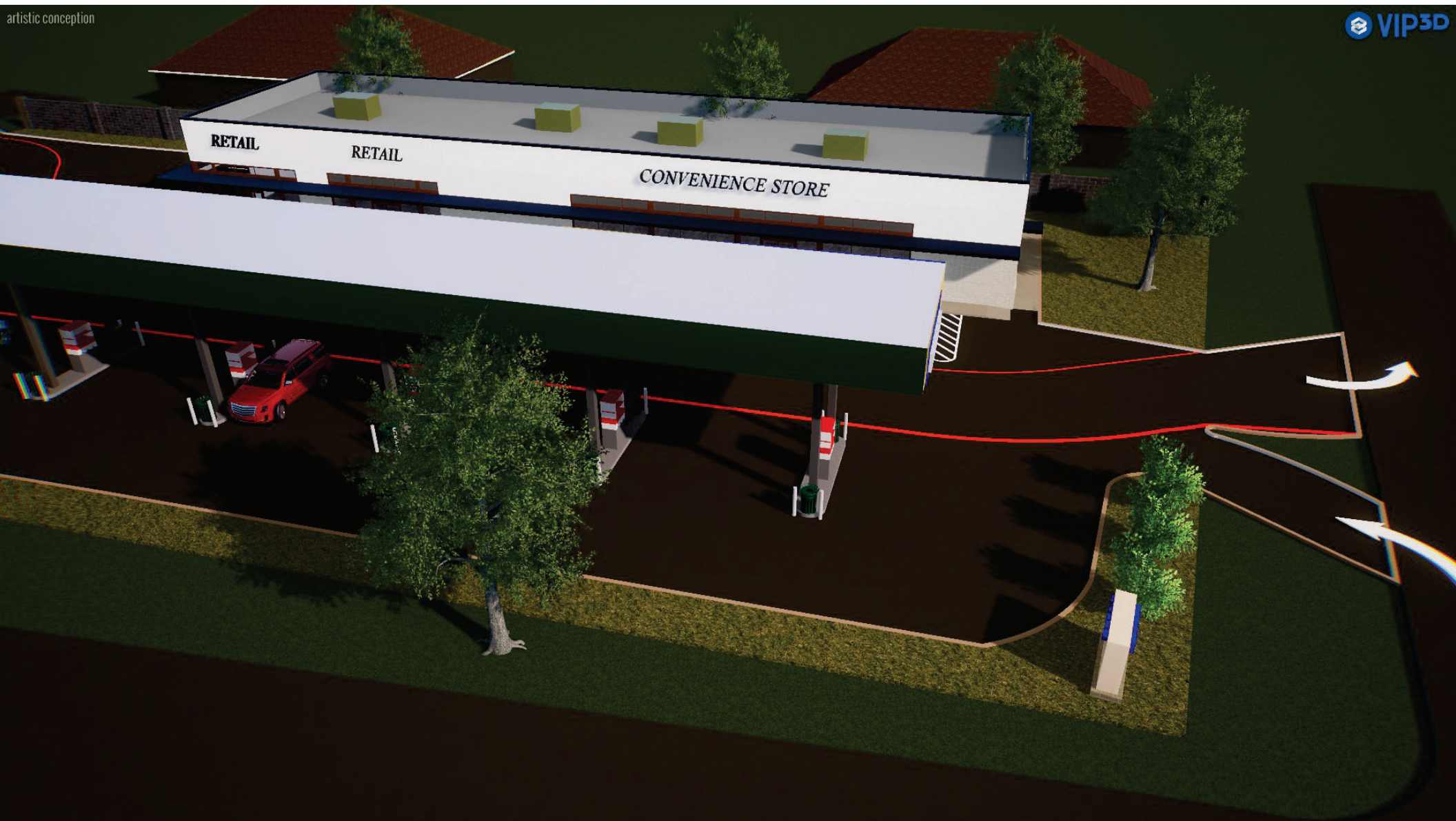






artistic conception

VIP3D



DOCUMENT NOT INCLUDED IN STAFF'S FINDINGS AND RECOMMENDATIONS

artistic conception



artistic conception

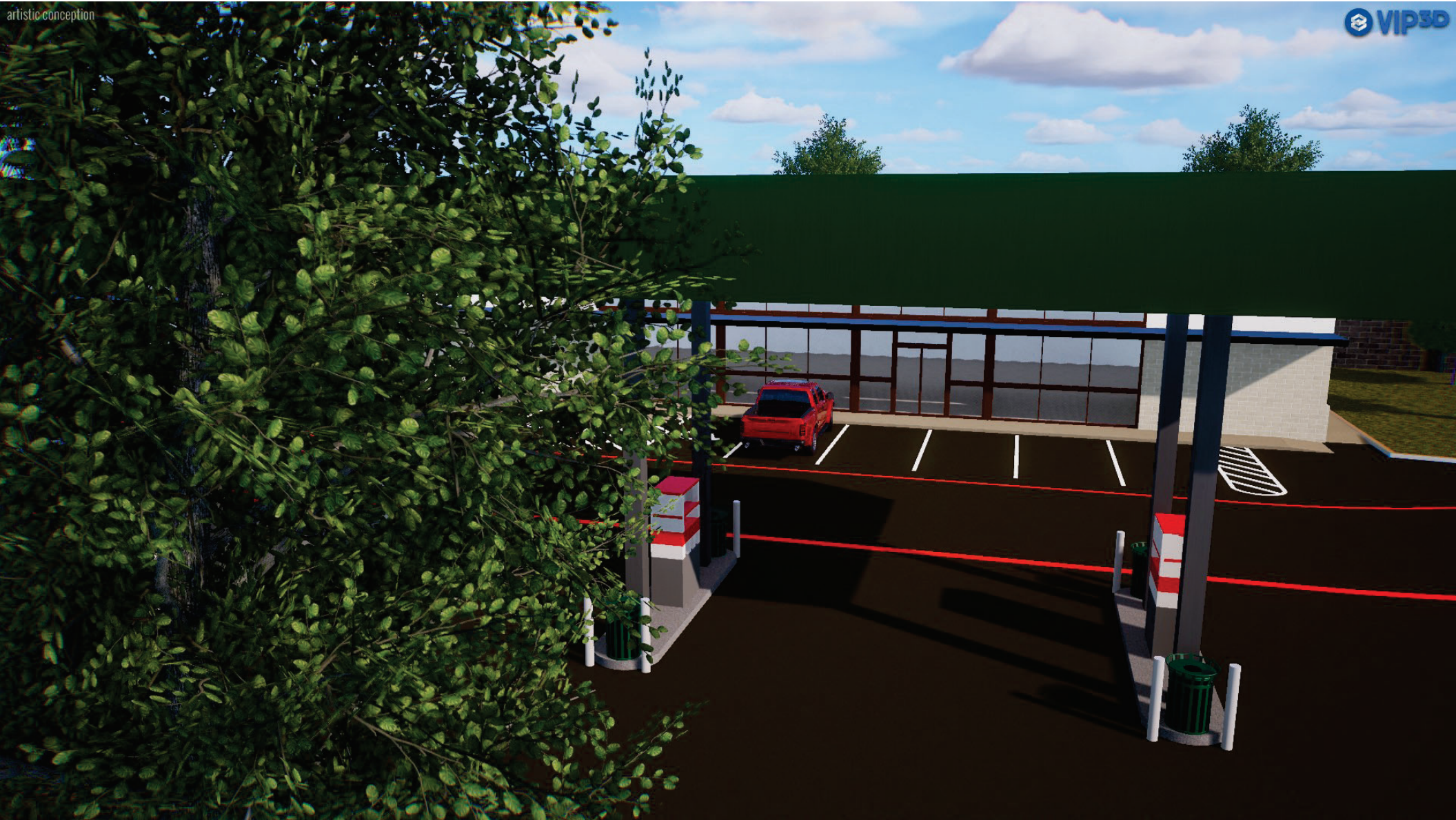
VIP3D



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artistic conception

VIP3D



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artistic conception

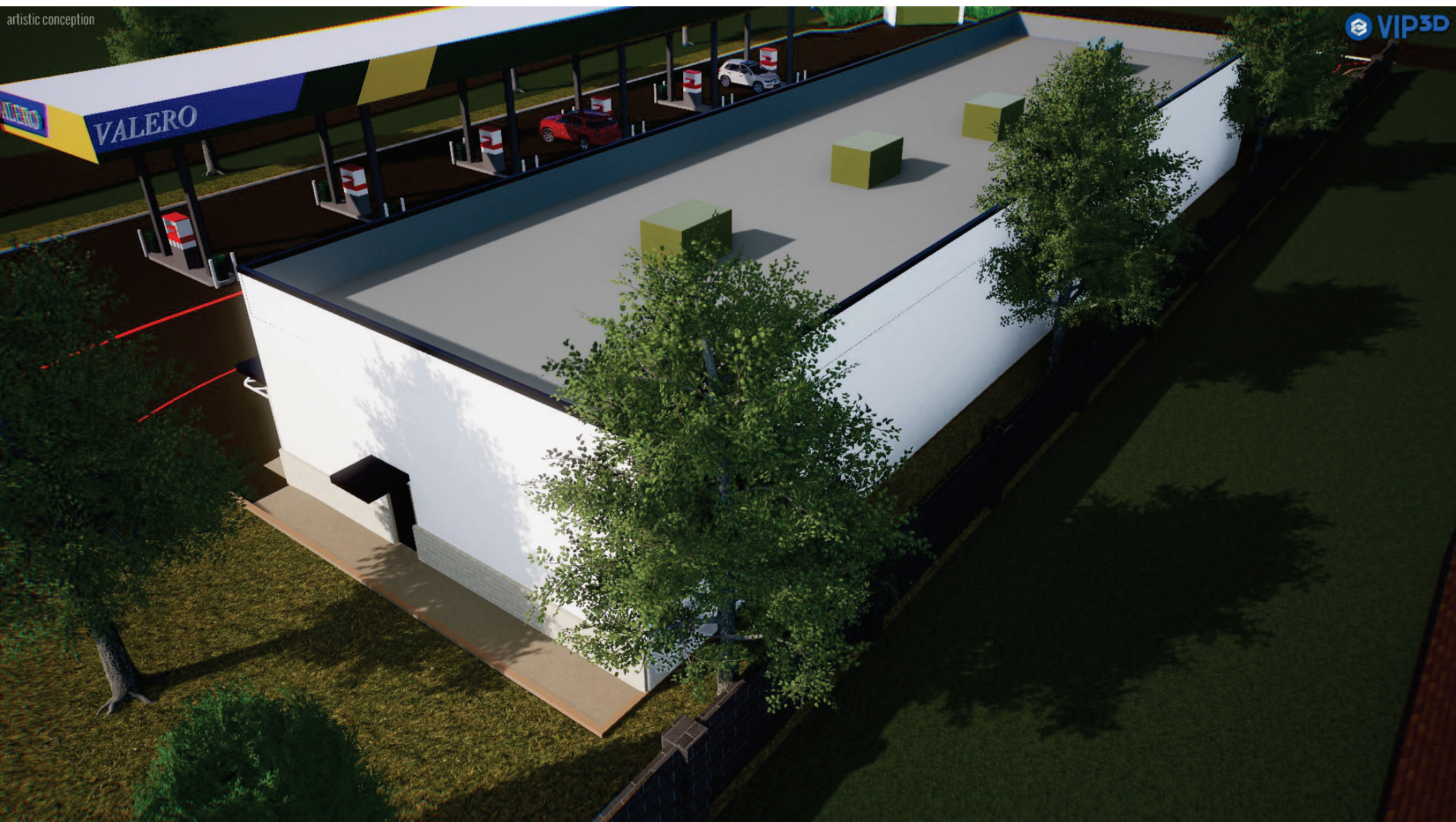
VIP3D



artistic conception



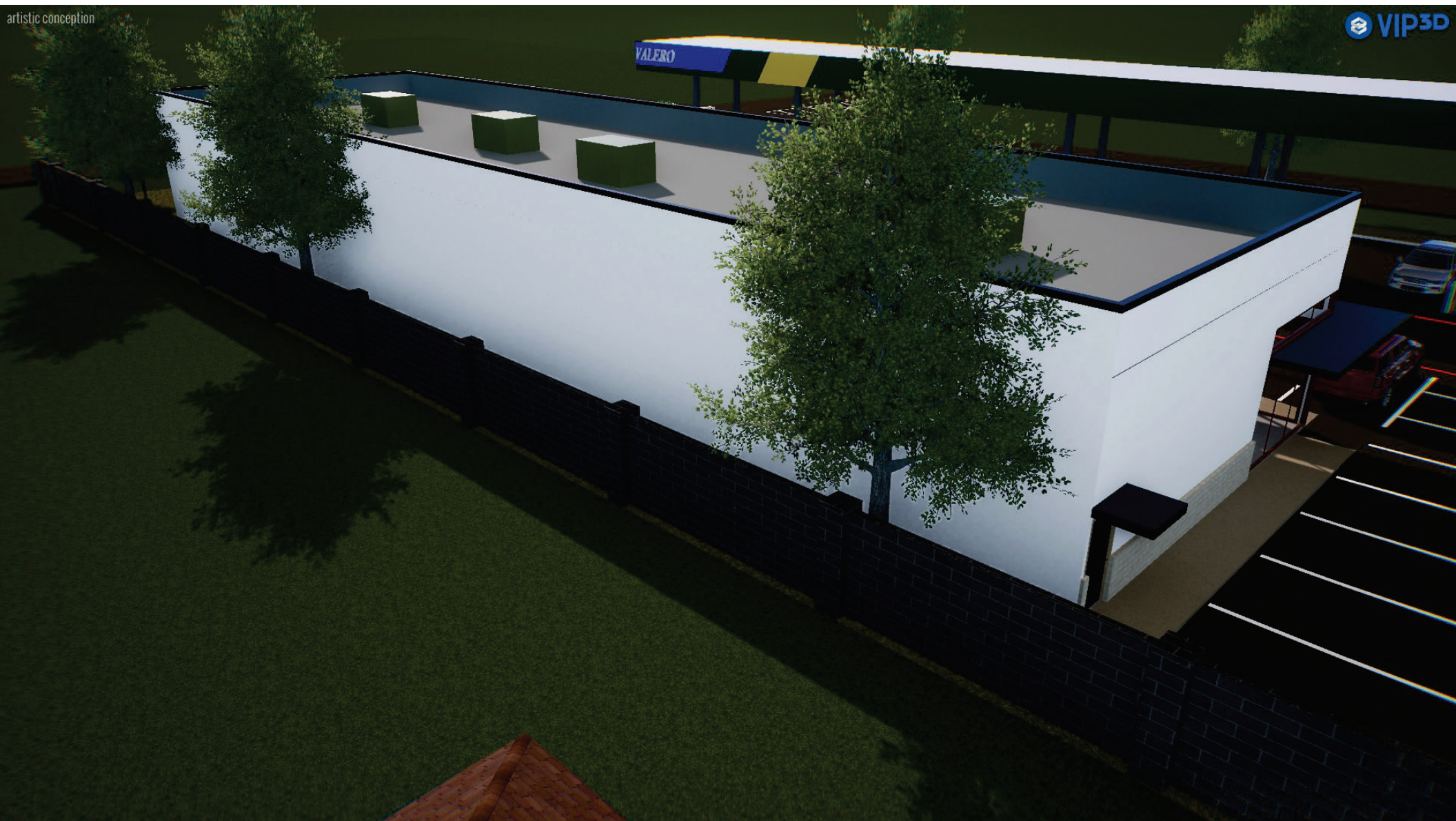
artistic conception



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artistic conception

VIP3D



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artistic conception

VIP3D



artistic conception



artistic conception

